



CHERRYWOOD

TOWN CENTRE (TC 1-4)
URBAN FORM DEVELOPMENT FRAMEWORK
Cherrywood Planning Scheme

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Cherrywood Urban Form Development Framework
was prepared by the Cherrywood Development Agency in consultation with the landowners, DLR Properties and Hines.

Document Design:
Cronin Designs

Chapter 1: Introduction

1 Introduction

1.1 Background

The Cherrywood Strategic Development Zone Planning Scheme was approved by An Bord Pleanála (ABP) in April 2014 in accordance with Section 169 of the Planning and Development Act 2000, as amended, subject to a number of modifications. Modification No. 1 of ABP specifies that it is a pre-requisite to the approval of any planning applications for the Town Centre, that following consultation with the relevant landowners, an Urban Form Development Framework (UFDF) be prepared by the Development Agency in accordance with the Cherrywood SDZ Planning Scheme. This requirement is set out in Section 6.2 of the Planning Scheme.

This UFDF has been prepared by the Cherrywood Development Agency further to an on-going collaborative process of engagement and consultation with the relevant landowners.

The UFDF provides comprehensive guidance on all key aspects of development within the Town Centre (TC1-TC4) and as such addresses the criteria as set out for inclusion by ABP (Section 6.2 items a-j). The UFDF is consistent with the approved Planning Scheme. Accordingly, the UFDF does not materially alter the Planning Scheme.

1.2 Vision

The vision, principles and design challenges for the Town Centre as set out in the Cherrywood Planning Scheme and outlined below, have informed the guiding principles of the UFDF.

Vision
<ul style="list-style-type: none">To create a sustainable place with a rich urban diversity, which respects its historical and natural setting while also facilitating innovation and creativity.
<ul style="list-style-type: none">To spatially develop a cohesive and diverse community with a strong identity and environmental integrity.
<ul style="list-style-type: none">To contribute to the economic growth of the County through the development of a vibrant economic community anchored around the Town Centre.
<ul style="list-style-type: none">To provide a safe and friendly environment where people can live, work and play within an envelope of sustainable, integrated transport with a primacy of soft modes of transport throughout.

Principles for Cherrywood
<ul style="list-style-type: none">To promote the growth of Cherrywood which enhances and supports balanced sustainable growth in the Greater Dublin Region and does not undermine the vitality and viability of other areas in the County and the Region.
<ul style="list-style-type: none">To create the framework for the development of a sustainable town and three villages with a supporting range of uses for the resident, working and visiting population.

<ul style="list-style-type: none">To link the area to its immediate hinterland and adjoining communities by restoring connectivity that has been severed by major roads.
<ul style="list-style-type: none">To work with the landscape by designing a form that is specific to Cherrywood, with a network of places each responding to its setting, landscape and climate.
<ul style="list-style-type: none">To balance the employment, commercial and retail base of Cherrywood with the future residential growth of the Plan Area.
<ul style="list-style-type: none">To create an environment that promotes / facilitates internal pedestrian and cycle movement meeting the requirements of Smarter Travel.

Town Centre Design Challenges
<ul style="list-style-type: none">To ensure the scale of retail development relates to the growth potential of the Plan Area.
<ul style="list-style-type: none">Relating development to the new ground level created by the Luas line.
<ul style="list-style-type: none">Bridging the Wyattville Link Road for pedestrians, cyclists and vehicles.
<ul style="list-style-type: none">The critical design and layout challenge is to provide an environment which overcomes the division caused to the Plan Area by the Wyattville Link Road in a manner that is pedestrian friendly and which enables the different activities of the Town Centre to interact. This reinforces the Smarter Travel policies in the Plan, the mixed-use nature of the Plan Area and the relationship between the Town Centre and the whole of the Plan Area.

1.3 Context & Scope

The scope of the UFDF is defined by modification No. 1 of the Cherrywood Planning Scheme as required by ABP, set out in Section 6.2 of the Planning Scheme and is as follows:

<p>‘To ensure a comprehensive approach to the layout and urban form of the Cherrywood town centre, it is a pre-requisite to the approval of any planning applications for the town centre that, following consultation with the relevant landowners, an Urban Form Development Framework be prepared by the Development Agency in accordance with the Cherrywood SDZ Planning Scheme.</p> <p>The purpose of the Urban Form Development Framework is to provide clarity and to assist the assessment of whether planning applications are consistent with the objectives of the Planning Scheme. Any development permitted in the town centre shall be in accordance with the Urban Form Development Framework.</p> <p>The preparation of the Framework shall take cognisance of the design approach set out in the submissions received by An Bord Pleanála on the</p>
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<p>27th day of February 2014 from BMA Planning and MÓLA Architecture on behalf of Cherrywood Properties, and from McCutcheon Halley Walsh and Scott Tallon Walker Architects on behalf of Dún Laoghaire-Rathdown Properties Limited.</p> <p>The level of detail provided by the Framework shall be at least that provided in the submission to An Bord Pleanála by Scott Tallon Walker Architects on behalf of Dún Laoghaire-Rathdown Properties Limited, on the 27th day of February 2014, which related to quadrant TC3. The Framework shall address the area identified as ‘town centre’ (TC 1-4) on Map 6.2 ‘Development Area 2 Cherrywood’ and shall, at a minimum, include the following:</p> <p>(a) Identification of:</p> <ul style="list-style-type: none">the principal focus and civic core of the town centrethe principal retail focus for quadrant TC1, andthe relationship between quadrant TC1 and Grand Parade, and their respective urban identities. <p>(b) a block layout for the town centre, illustrating urban blocks and urban grain,</p> <p>(c) height, scale and massing, and building typologies,</p> <p>(d) finished floor levels for buildings, including identification of entrance level(s), and finished floor levels for roads and footways, and in particular, the horizontal and vertical connections between Grand Parade and the surrounding streets/buildings.</p> <p>(e) the location, design and treatment of squares, civic spaces and pocket parks, and the public realm generally,</p> <p>(f) provision for facilities including a community facility, places of worship, a library, and a primary care unit, in accordance with Objective DA11</p> <p>(g) the treatment of the Grand Parade generally, including the proposed approach to the crossing of the Wyattville Link Road, demonstrating how an attractive and welcoming urban environment can be maintained, having regard to its important transport function as set out in the Planning Scheme,</p> <p>(h) provision for a separate pedestrian/ cycle bridge across the Wyattville Link Road, connecting quadrants TC1 and TC3, as shown in Map 6.2 ‘Development Area 2 Cherrywood’, and provision for the sequencing of this development.</p> <p>(i) identification of vehicular and pedestrian routes within the town centre, and associated finished levels and access points, including car park access points, including section drawings and</p> <p>(j) identification of the car parking spaces to be provided, the location, access and usage of car parks, including shared use car parks, and any on-street parking.</p> <p>The framework shall not materially alter the Planning Scheme.</p> <p>In the event of disagreement between the parties on any matters within the Framework, the matter in question shall be referred to An Bord Pleanála for determination’.</p>

1.4 UFDF Response to the Requirements of the Planning Scheme

Section 6.2 ‘Development Area 2: Cherrywood’ of the Planning Scheme outlines the minimum requirements that the UFDF shall address as directed by An Bord Pleanála. These together with how the UFDF responds to these requirements are summarised in Table 1 below.

Table 1 UFDF Planning Scheme Requirements and Response	
UFDF Planning Scheme Requirement	Comment
The Framework shall address the area identified as ‘town centre’ (TC 1-4) on Map 6.2 ‘Development Area 2 Cherrywood’ and shall, at a minimum, include the following:	
(a) Identification of:	
- the principal focus and civic core of the town centre,	Cherrywood Square is the principal focus and civic core of the Town Centre identified in Section 2.4 of the Land Use Strategy, Section 4.2 Primary Civic Spaces, Map 6 Public Realm Strategy, Map 7 Landscape Master Plan, and Figures 17 and 18a & 18b.
- the principal retail focus for quadrant TC1, and	The principal retail focus is located in TC1, focused primarily around Town Square (East & West) as illustrated in Section 2.3 of the Land Use Strategy, Map 6 Public Realm Strategy and Map 7 Landscape Master Plan.
- the relationship between quadrant TC1 and Grand Parade, and their respective urban identities,	Section 3.1.1 ‘Building a New Ground Level’ and Section 3.2.5 ‘Reconciling Ground Levels and Block Layout’ describes the relationship between the four Town Centre quadrants TC1,TC2,TC3,TC4 and Grand Parade. It is proposed that the buildings closest to the Grand Parade will follow the level of the Luas line and provide direct access and active frontage to Grand Parade.
(b) a block layout for the town centre, illustrating urban blocks and urban grain,	Section 3.2 ‘Block Layout, Urban Grain & Design Approach’ and Map 3 illustrates the proposed block layout and urban grain for each of the Town Centre quadrants based on the detailed design studies undertaken to date.
(c) height, scale and massing, and building typologies,	Information on building height scale and massing and building typologies is presented in Section 3.3 ‘Building Height, Scale and Massing’ and on Map 5. These are based on the concept designs from the various architects within the Design Team for Cherrywood Town Centre.
(d) finished floor levels for buildings, including identification of entrance level(s), and finished levels for roads and footways, and in particular, the horizontal and vertical connections between Grand Parade and the surrounding streets/buildings,	Grand Parade provides “fixed” levels along its entire length and the “tie-in” levels to the main streets and the general levels of the streets/ footways and buildings. These levels are identified in Section 3.2.4 ‘Reconciling Ground Levels and Block Layout’ and on Map 4.
(e) the location, design and treatment of squares, civic spaces, pocket parks, and the public realm generally,	Grand Parade provides 3 no. ‘Primary Civic Spaces’ along its length within the Town Centre at Cherrywood Square, Wyattville Link Plaza and Bride’s Glen Square, as identified in Section 4.2. ‘Secondary Civic Spaces’ are proposed within the principal retail focus in TC1, namely Town Square East and West and a new civic space is centrally located within TC3, all identified in Section 4.3. General proposals (Landscape Masterplan) are presented for the treatment of civic spaces, streets, private residential streets, courtyards and roof gardens and terraces to guide detailed design. These are identified in Section 4.0 The Public Realm Strategy, specifically Sections 4.1 Grand Parade, 4.2 Primary Civic Spaces, 4.3 Secondary Civic Spaces, 4.4 Streets-Pedestrian & Cycle, 4.5 Materials, Finishes & Street Furniture in the Public Realm, 4.6 Communal Amenity Space and 4.7 Micro Climate, Map 6 Public Realm Strategy and Map 7 Landscape Master Plan.
(f) provision for facilities including a community facility, places of worship, a library, and a primary care unit, in accordance with Objective DA11,	Section 2.1 and Map 2 identifies the distribution of land uses across the 4 no. Town Centre quadrants. Community uses are delineated and will include the provision of a library and primary care centre within TC1 and a minimum of one community centre and a place of worship within either TC1 and/or TC3. Details of these are outlined in Section 2.4.
(g) the treatment of the Grand Parade generally, including the proposed approach to the crossing of the Wyattville Link Road, demonstrating how an attractive and welcoming urban environment can be maintained, having regard to its important transport function as set out in the Planning Scheme,	Section 4.0 The Public Realm Strategy, specifically Sections 4.1 Grand Parade, 4.2 Primary Civic Spaces, 4.3 Secondary Civic Spaces, 4.4 Streets, 4.5 Materials and Finishes in the Public Realm, 4.6 Communal Amenity Space and 4.7 Micro Climate and Map 6 Public Realm Strategy and Map 7 Landscape Master Plan clearly identifies the proposals for the design and treatment of Grand Parade and the provision of the Wyattville Link Plaza to be located at the bridge crossing . Details are provided in this section of the environment which will be created at this location and mitigation measures to address safety/ comfort of pedestrians.
(h) provision for a separate pedestrian/cycle bridge across the Wyattville Link Road, connecting quadrants TC1 and TC3, as shown in Map 6.2 ‘Development Area 2 Cherrywood’, and provision for the sequencing of this development.	A pedestrian & cycle bridge crossing the Wyattville Link Road is identified in Section 4.3.1 TC3 Civic Square and 5.1 Pedestrian Movement Strategy and 5.2 Cycle Movement Strategy linking quadrants TC1 and TC3.
(i) identification of vehicular and pedestrian routes within the town centre, and associated finished levels and access points, including car park access points, including section drawings and	Section 5.0 Access and Movement Strategy includes details on pedestrian movement, cycle movement, public transport movement, vehicular access and circulation, car parking provision and car parking access and circulation and servicing and HGV access . This strategy identifies a network of pedestrian and cycling routes, vehicular routes and access points, the interconnectivity between the 4 no. quadrants and the surrounding road network.
(j) identification of the car parking spaces to be provided, the location access to and usage of car parks, including shared use car parks, and any on-street parking.	The Car Parking Strategy, including car parking provision, access and circulation is outlined in Section 5.4.2 and 5.4.3.

In the preparation of the UFDF, the Town Centre quadrants have been considered in the context of the proposals for the wider area including existing development at the Cherrywood Business Park, existing residential developments at Tully Vale and Druids Valley and permitted development to date for the roads infrastructure and public parks.

The Urban Form Development Framework will be the basis on which planning applications for the Town Centre will be assessed throughout the duration of the Planning Scheme. In this regard, the UFDF has been prepared to ensure consistency with the Cherrywood Planning Scheme. It sets out clear principles and guidance to assist in the assessment of planning applications, whilst also providing a level of flexibility to support the delivery of the Town Centre. Flexibility may be applied in the interpretation of standards so that they do not become inflexible obstacles to the achievement of the objectives of the UFDF and the Planning Scheme.

It is acknowledged that at detailed design or assessment stage precise adherence to the UFDF may not be feasible or practicable and the need for minor variations from the layout as illustrated in the UFDF may arise with regard to certain elements - such as ground levels, floorplates, basement layouts, block layout or building form- subject to overall compliance with the key principles in relation to the network of streets and spaces, the public realm, permeability, residential amenity, daylight and sunlight access, overall urban structure and the movement and access strategy.

Any proposal which conflicts with the key principles and overall urban structure of the UFDF and spirit of the direction of An Bord Pleánala as set out in Section 6.2 of the Planning Scheme, will be deemed not to be in compliance with the UFDF.

Some of the illustrations contained within the UFDF are indicative and are shown for illustrative purposes. Other innovative solutions which achieve the key objectives of the Planning Scheme and comply with the UFDF could be considered at the discretion of the Planning Authority.

1.5 Structure of the UFDF

For ease of reference and to ensure consistency with the Planning Scheme, the Structure of the UFDF is based on the requirements of Section 6.2 (a) – (j) of the Planning Scheme:

Section 2.0 Land Use Strategy for the Town Centre defines the distribution of land uses across the 4 no. quadrants and the principal focus and civic core and the principal retail focus in accordance with the requirements of Section 6.2 (a) & (f) of the Planning Scheme.

Section 3.0 Urban and Built Form and Layout outlines the urban form and urban grain which is anticipated for the Town Centre. This section addresses the requirements of Section 6.2 (a), (b), (c), (d) & (e) of the Planning Scheme.

It first addresses the issue of ground levels and the design challenges which are faced as a result of the requirement to bridge the divide created by the Wyattville Link Road and to meet the elevated level of the Luas line. The design shall ensure ease of pedestrian movement by either stepping the buildings up from the edge of the Town Centre to the level of the Luas line or by building the Town Centre on a podium that rises to the level of the Luas. This section then defines a block layout, which identifies the spaces between the blocks to form a hierarchy of civic spaces and a network of streets which all form part of a Public Realm Strategy. The next challenge was to reconcile the new ground levels with the block layout and building height, so Section 3 proceeds with defining building height and scale across the Town Centre including the illustration of a series of upward modifiers to articulate what are considered to be important locations. Section 3 continues to also ensure that quality is established within the design of the Town Centre, in terms of buildings, surfaces and treatments.

Section 4.0 The Public Realm Strategy identifies a hierarchy of civic spaces and the street network which connects them, in particular and including the main spine of the Town Centre; Grand Parade. The UFDF illustrates in detail how this street shall ‘feel’ from its function to its surface treatment, all in accordance with the requirements of Section 6.2 (f) & (g) of the Planning Scheme.

Section 5.0 The Access and Movement Strategy defines how people will access and move around the Town Centre by all modes of transport, including a servicing and HGV access and car parking provision, all designed with the key objective in mind - ‘sustainable travel’ all in accordance with the requirements of Section 6.2 (h) (i) & (j) of the Planning Scheme.

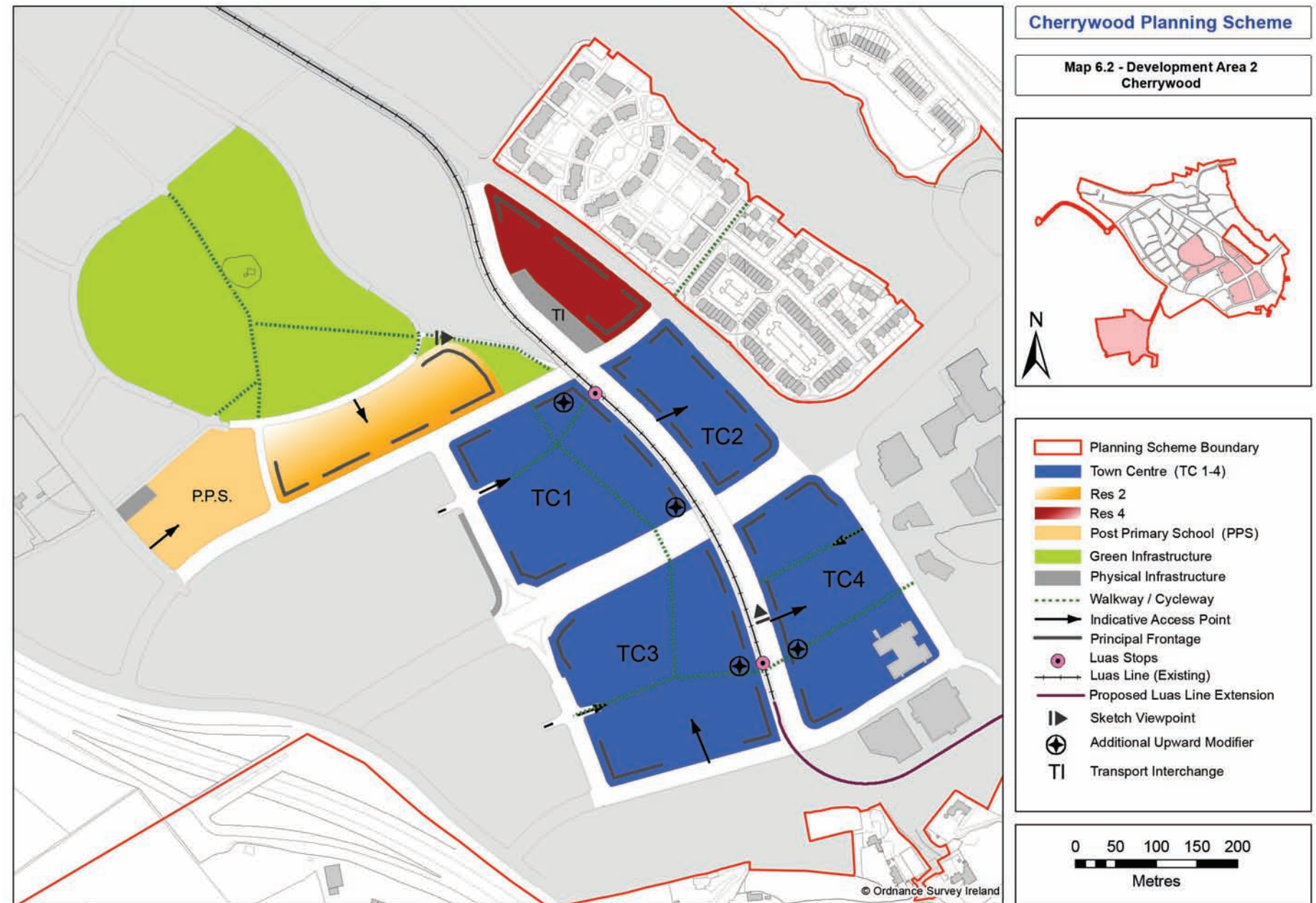
Section 6.0 Delivery & Implementation sets out the proactive role the Development Agency and Dun Laoghaire-Rathdown County Council will play in realising the vision for Cherrywood Town Centre. It also sets out implementation, monitoring and review mechanisms.

Chapter 2: Land Use Strategy

2 Land Use Strategy

The Land Use Strategy outlined in the UDF is provided in accordance with Section 6.2 (a) and (f) of the Cherrywood Planning Scheme which requires to identify the principal focus and civic core of the Town Centre, the principal retail focus in quadrant TC1 and the relationship between Grand Parade and their respective identities. This Section outlines the distribution of land uses across the 4 no. quadrants in line with the quanta identified in the Planning Scheme.

The Town Centre is subdivided into 4 no. quadrants; TC1, TC2, TC3 and TC4 as identified on Map 1 'Extract from the Planning Scheme'.



Map 1: Extract from the Planning Scheme (Map 6.2 Development Area 2: Cherrywood)

2.1 Land Use Distribution

The Planning Scheme (as amended 2017) sets out a total approved floor area range (sq.m) for the Town Centre as a whole, defined by the minimum total floor space and minimum number of residential units (circa) permissible and the maximum total floor space and maximum number of residential units (circa) permissible. This quantum is allocated to 5 no. land use categories: retail; non-retail; residential; high intensity employment (HIE); and community. These are identified in Table 6.2.1 'Development Type and Quantum for Development Area 2' of the Planning Scheme. Furthermore, Table 6.2.2 'Breakdown of Development Quantum for sites TC1, TC2, TC3 and TC4' defines this quantum and land use distribution across each of the 4 no. Town Centre quadrants. The Land Use Strategy contained within Section 2.0 of the UDF and illustrated on Map 2 was developed having regard to the land use quanta and allocation and the future form of the Town Centre as defined in the extract from the Planning Scheme below:

Cherrywood Town Centre will accommodate the greatest concentration of development within the Plan Area. The Town Centre area is a mixed use area, which will provide for a range of uses including: residential, high intensity employment, retail, leisure, cafés and restaurants, ensuring that there is activity throughout the day and into the evening. The mix of interdependent land uses will contribute to the sense of place and identity.

Map 2 provides a conceptual distribution of land uses within the Town Centre. The uses create quite distinct character quarters. TC1 provides both the principal retail focus, with a significant proportion of the total retail quantum and also the principal focus and civic core in close proximity to the Cherrywood Luas stop and the Transport Interchange, entrance to Tully Park and at the hub of civic and community facilities as required by Section 6.2 (a) of the Planning Scheme. TC2 caters for predominantly residential uses with active uses animating Grand Parade. TC3 allows for a broad mix of uses providing a significant level of high intensity employment focussed around a large public area. This central area of TC3 is a focal gathering point with a mix of uses including community, retail, non-retail, residential and high intensity employment (HIE), all of which contribute to a high level of activity in this part of the Town Centre throughout the day and in the evening. The ground levels of the residential buildings include elements of retail, community and non-retail uses. TC4 is predominantly residential with employment and non-retail uses with a civic focus around the Bride's Glen Luas stop.

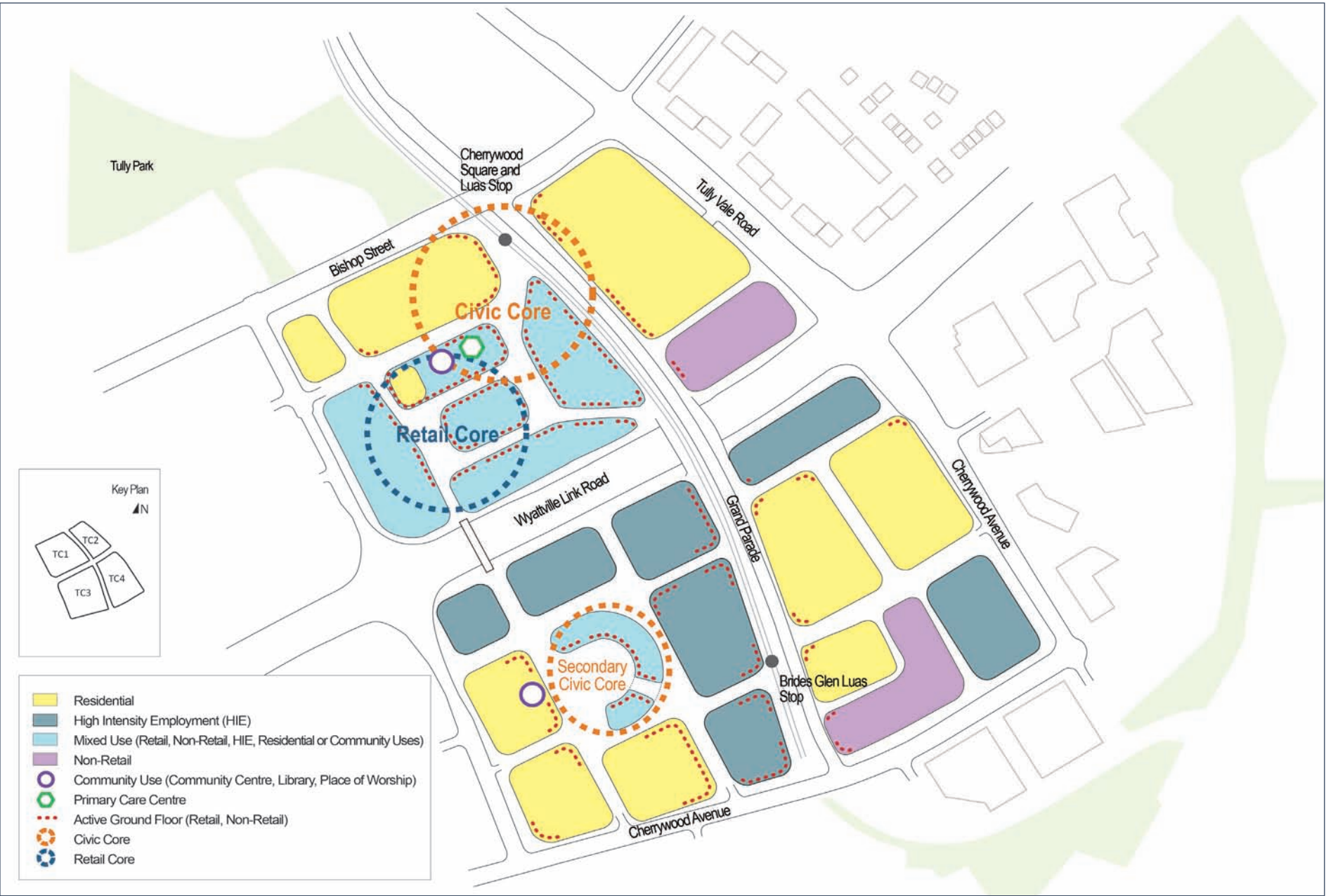
It is relevant to note that the Planning Scheme provides for an allowance of 10% transfer of uses between the 4 no. quadrants in the Town Centre to give flexibility, whilst maintaining the overall quanta in Tables 2.1 and Tables 6.2.2.

2.2 Active Uses and Frontages

Throughout the Town Centre the distribution and nature of uses, together with the design of the layout and built form shall contribute towards creating streets that are a focus of activity and provide a good level of natural surveillance to make people feel safe and comfortable.

Map 2 illustrates how, at the ground floor level of the primary land uses, both active retail uses (such as retail services) and non-retail uses along principal frontages shall be provided to achieve this level of animation.

In addition, other uses, including residential, community, employment and other non-commercial uses can also provide activity and passive surveillance of streets. Where possible building design shall avoid the use of plinths along streets, providing direct access and the avoidance of blank elevations at street level (See also Sections 3.2.1 and 3.2.2).



Map 2: Land Use Strategy

2.3 Principal Retail Focus

In terms of retail allocation, Table 6.2.1 of the Planning Scheme contains a breakdown of categories of retail use including convenience, comparison and retail services. In accordance with the requirement of Section 6.2 (a) of the Planning Scheme, the UDF identifies the principal retail focus within TC1. Approximately one third of TC1 is retail use, providing a mix of traditional convenience and comparison retail uses and complimentary retail services. The principal retail focus is positioned centrally within TC1 backing onto the Wyattville Link Road, fronting onto Grand Parade and opening up into a network of spaces including the Town Square (East and West), the wide pedestrian street which runs east to west across TC1, namely Civic Street and Cherrywood Square, and overlaps with the principal focus and civic core.

As illustrated on Map 2 retail uses are also distributed throughout the 4 no. quadrants, annotated as 'Active Ground Floor Uses'. The distribution of uses which animate and enliven streets throughout the day and evenings shall be located along Grand Parade, other primary streets and around the main spaces which also provide areas for these uses to spill into. 'Active Ground Floor uses' may relate to non-retail uses.

2.4 Principal Focus and Civic Core

In accordance with Section 6.2 (a) of the Planning Scheme 'to identify a principal focus and civic core' and (f), to identify 'the provision of facilities including a community facility, places of worship, a library, and a primary care unit in accordance with Objective DA16, 17 and 18 within the Planning Scheme', the UDF identifies the principal focus and civic core within TC1, focused around the provision of a community facility/ library, place of worship and primary care unit, in the vicinity of Cherrywood Square at the Gateway to the mixed used area of TC1. The civic core encompasses Cherrywood Square at the Cherrywood Luas stop, connected physically and visually to the entrance to Tully Park, the Transport Interchange and the 'Civic Street'. It is a place of significance and is articulated by taller buildings and dominated by the main public and civic uses and focal buildings such as the library, with the addition of retail and non-retail uses which provide activity during the day and evening. The area and the buildings surrounding it shall be designed so as to have presence and a quality that establishes civic pride.

A secondary civic area is located within TC3 which provides a centrally located 'hub' enclosed by a mix of uses including employment, non-retail, retail, community and residential uses and connected by links to the remainder of the Town Centre.

2.4.1 Library

A Library of approximately 1,500sqm in accordance with Objective DA17 and Section 2.3.4 of the Planning Scheme, shall be provided along 'Civic Street' located in TC1. The design of this facility shall provide a landmark community building which is visually distinct, highly visible and attracts visitors. Internally it shall be bright and welcoming and be fully accessible. It shall act as a community and cultural hub providing study and reference facilities, music and exhibition rooms, and hold the key to the heritage and identity of the area, in particular providing where required, a Heritage Area which can allow for the display of artefacts on the heritage of Cherrywood, in particular Tully Church.

2.4.2 Community Facility

A minimum of 1 no. community facility of approximately 250sqm with an appropriate floor to ceiling height shall be provided within TC3 in accordance with Objective DA16 and Section 2.3.4 of the Planning Scheme. Consideration shall be given to the provision of a 'Community Hub' in TC1 to incorporate both the library and community facilities in the short term. A 'hub' provides a focus in the early stages of a new community's development, creating footfall and a destination that will encourage and entice business and retail into the area which traditionally arrives much later. This 'hub' should be fully accessible and flexible in terms of the adaptability of the rooms in order to serve a multiplicity of activities and user groups.

2.4.3 Primary Care Unit

A Primary Care facility shall be provided within quadrant TC1, located along 'Civic Street' at the centre of the civic core, in accordance with Objective DA18 of the UFDF and Section 2.3.6 of the Planning Scheme.

2.5 Non-Retail

Non-retail uses are provided along Grand Parade and dispersed along the main pedestrian circulation routes and adjacent to the Luas stops throughout all 4 no. quadrants, all at ground floor level (below the primary uses) providing direct street level access contributing to the animation and activation of streets and spaces. The high profile corner of TC2 at Junction A is chosen as a suitable location for a hotel use and a viable option for the location of non-retail uses within this quadrant. The central area of TC3 provides a mix of uses including non-retail uses within this secondary civic core. The south west corner of quadrant TC4 provides a block of non-retail uses.

2.6 Residential

Approximately one third of TC1 is allocated to residential use in the form of high density apartment development. This shall be located mainly in the quieter Bishops Street side of TC1 closest to Tully Park and shielded from the Wyattville Link Road. TC2 is predominantly (three quarters) residential. The design of this quadrant shall locate the residential use to its north, remote from the Wyattville Link Road, in close proximity to the residential plot north of Bishops Street on the edge of the Town Centre. TC3 has a significant (one quarter) residential allocation. This is to be provided in the form of apartments located mainly towards the southern/ south- western corner of the plot where they can benefit from a favourable southerly aspect and a more suitable environment generally. TC4 provides a significant proportion of the residential quantum within the Town Centre, contributing to approximately half of this quadrants total land use quanta. The residential uses are to be provided centrally within the quadrant extending from Grand Parade to Cherrywood Avenue. The design and layout of the Town Centre, shall be required to demonstrate that residential amenity is protected.

2.7 High Intensity Employment (HIE)

Within TC1 high intensity employment uses are to be located on upper levels of the buildings adjacent to the Luas line. TC3 is predominantly (two thirds) high intensity employment. This shall be located closest to the Wyattville Link Road and along Grand Parade. TC4 has an existing office building (Block H) which is to be retained and integrated into the development of TC4. The remainder of the HIE allocation in TC4 shall be located on the prominent corner at Junction A of the Wyattville Link Road and will form a landmark and a buffer to the residential elements within TC4.

Chapter 3: Urban & Built Form & Layout

3 Urban & Built Form & Layout

This section of the UDF defines the urban and built form and block layout for Cherrywood Town Centre. The principles of this follow those specifically contained in Section 6.2 (a) (b) (c) and (d) of the Cherrywood Planning Scheme and its requirement to identify the relationship between Quadrant TC1 and Grand Parade and their respective urban identities, a block layout for the Town Centre, illustrating urban blocks and urban grain, height, scale and massing, and building typologies, finished floor levels for buildings, including identification of entrance level(s), finished levels for roads and footways, and in particular, the horizontal and vertical connections between Grand Parade and the surrounding streets/buildings. The future urban form is also based around the requirements of Section 6.2 (e), (g) (h) and (i) and reflects the principles of those as detailed in Section 4.0 Public Realm Strategy and Section 5.0 Access and Movement Strategy. The urban form and layout for the Town Centre has been formulated and predicated on Objectives DA10, DA11, and DA13 and the challenge created by the existing form as outlined in the extract from Section 6.2 'Future Form' of the Cherrywood the Planning Scheme below:

The challenge with the Town Centre and the benefit of having it straddle the Wyattville Link Road is that there is an opportunity for it to create its own character, building on its wider setting. The Town Centre will bridge the divide created by the Wyattville Link Road, will create a Centre where all the different land uses are catered for and the needs of the residents and employees are met. It is the centre of economic, social and retailing activity within the Plan Area. Its design and layout must embrace these functions by developing spaces of a quality that attracts people to them and through them to allow a sustainable level of footfall to be achieved that will enhance the viability of the area and contribute to a feeling of security. This could be approached in a number of ways including:

- Designing the Town Centre link so that it will encourage and channel movement across the Wyattville Link Road.
- Designing the layout of the Town Centre so that ease of pedestrian movement to the Town Centre link is a primary design consideration. This could be achieved in a number of ways including:

Stepping the buildings up from the perimeter of the Town Centre plots to the level of the Luas line and the pedestrian crossing of the Wyattville Link Road, so that across the Town Centre the buildings immediately adjoining the Town Centre link and Luas line are at grade;

Or

Building the Town Centre on a podium that rises to the level of the Luas line. The treatment of plot edges will require considered design solutions.

For this elevated environment to be successful it will be perceived by the pedestrian as a real and natural place. As such the interface between the elevated Town Centre link and the adjoining ground should be designed to protect from the visual and auditory effects of traffic on the Wyattville Link Road.

3.1 Ground Levels & Design Approach

3.1.1 Building a New Ground Level

The Planning Scheme identifies that the main issue informing the design of the Town Centre is the requirement for the built form to respond to the levels created by the arching vertical and horizontal alignment of the Luas line and the need for the ground levels of the new Town Centre to address this important structuring element.

In addition, the severing effect of the Wyattville Link Road which connects the M50 with the N11 strategic routes, needs to be addressed to ensure that the pedestrian and cyclist, in particular, feel comfortable in the transition from one side of the Wyattville Link Road to the other and as such across the Town Centre as a whole. In terms of responding to the issue of ground levels, the critical design challenge is to ensure the Town Centre development provides an environment which is inclusive and sustainable.

In this regard the UDF is required to identify finished floor levels for buildings, including identification of entrance level(s), finished levels for roads and footways, and in particular, the horizontal and vertical connections between Grand Parade and the surrounding streets/buildings.

To achieve the required vertical relationship with the Luas line, a combination of building a podium and the stepping up of buildings will be required. These approaches are demonstrated below and in Section 3.2.5 'Reconciling Ground Levels and Block Layout', where the approach in quadrant TC3 differs somewhat to that in TC1, TC2 and part of TC4.

3.1.2 Design Approach

TC1 and TC2

The integrated nature of the masterplan and design concepts for TC1 and TC2 allows for these quadrants to be developed using the podium type approach which addresses the level of the Luas line as illustrated in Figure 1.

Building on podium will result in the creation of a 2-3 level basement area providing elements of retail and enabling all car parking, servicing, plant and other ancillary accommodation to be located below podium thus facilitating the creation of high quality pedestrian and public realm areas above in a largely traffic free environment. The integration of the below ground areas has the benefit of allowing the car parking and access strategies to rationalise servicing and parking movements and provide a more efficient vehicular access and circulation strategy.

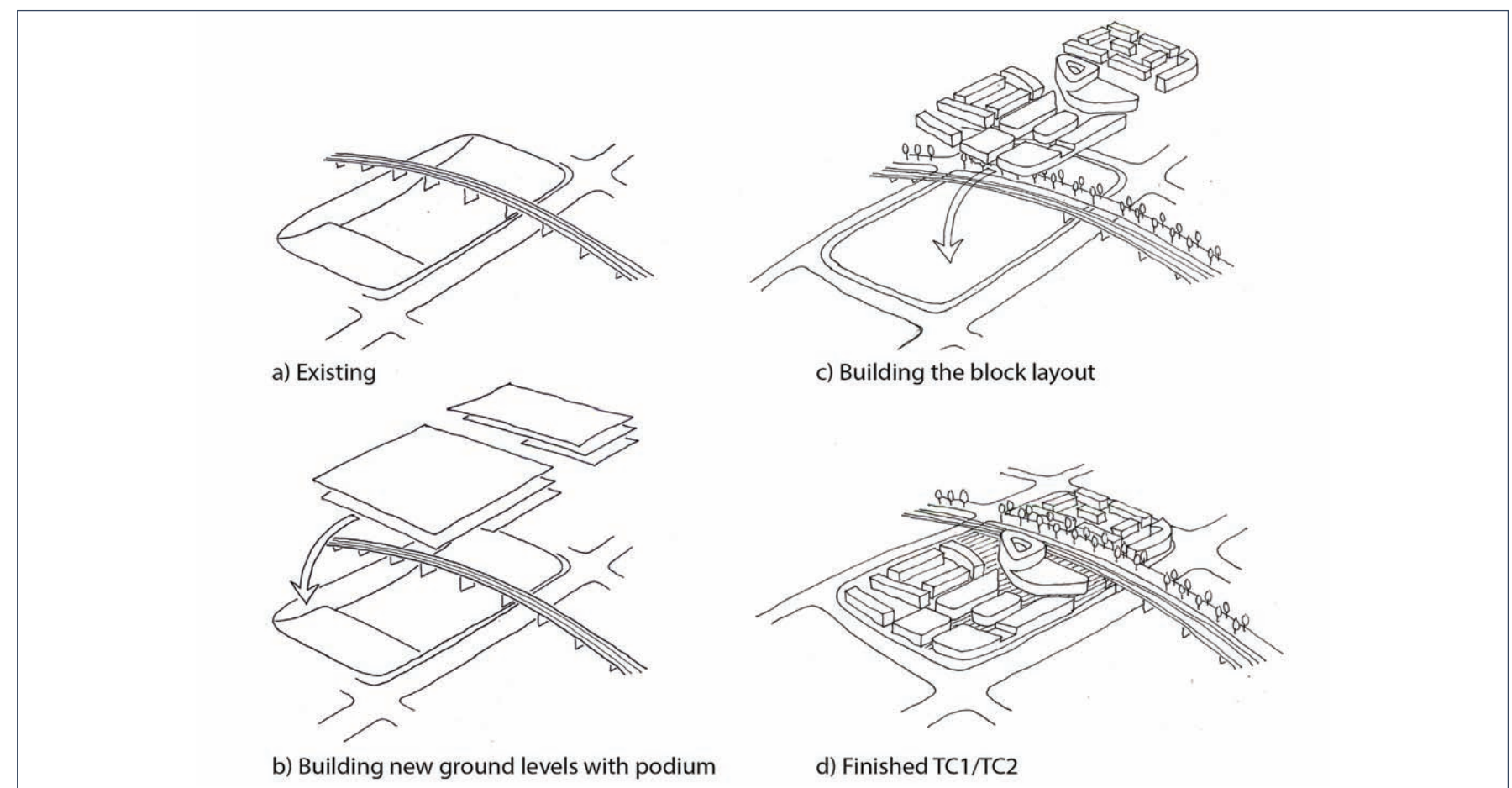


Figure 1: Building a New Ground Level – Concept Diagram showing TC1 and TC2

TC3

The proposed ground levels for quadrant TC3 need to respond to the existing ground levels, the levels of Grand Parade and the levels required to cross the Wyattville Link Road (Map 3 'Block Layout, New Ground levels and Principal Frontages' and Figure 2).

The ground plane therefore needs to step up to meet these conditions, with two principal ground planes being at circa +52m OD in the central and eastern part of the quadrant and +56m OD around the western and southern area. The ground plane slopes along Grand Parade following the gradient of the Luas line.

The significant level distance of the Luas crossing (+54.5m OD) over the Wyattville Link Road (approx +46m OD) is accommodated by providing a lower level of accommodation at +48m OD at the northern corner adjacent the Luas crossing and Wyattville Link Road.

This approach utilises the existing excavated area below the new ground plane to provide car parking and servicing beneath. There is no vehicular interconnection between TC3 and TC4 below the Luas line and servicing and car parking for these blocks will be provided independently.

TC4

The design of quadrant TC4 shall utilise the excavated area beneath the Luas line level to provide 2-3 levels of car parking towards the western part of the quadrant. As the development moves east of the quadrant across the internal, at-grade street, towards the existing Business Park and Cherrywood Avenue, most of the development here shall be at existing ground level, with the exception of localised excavation required for a below grade basement for car parking and servicing.

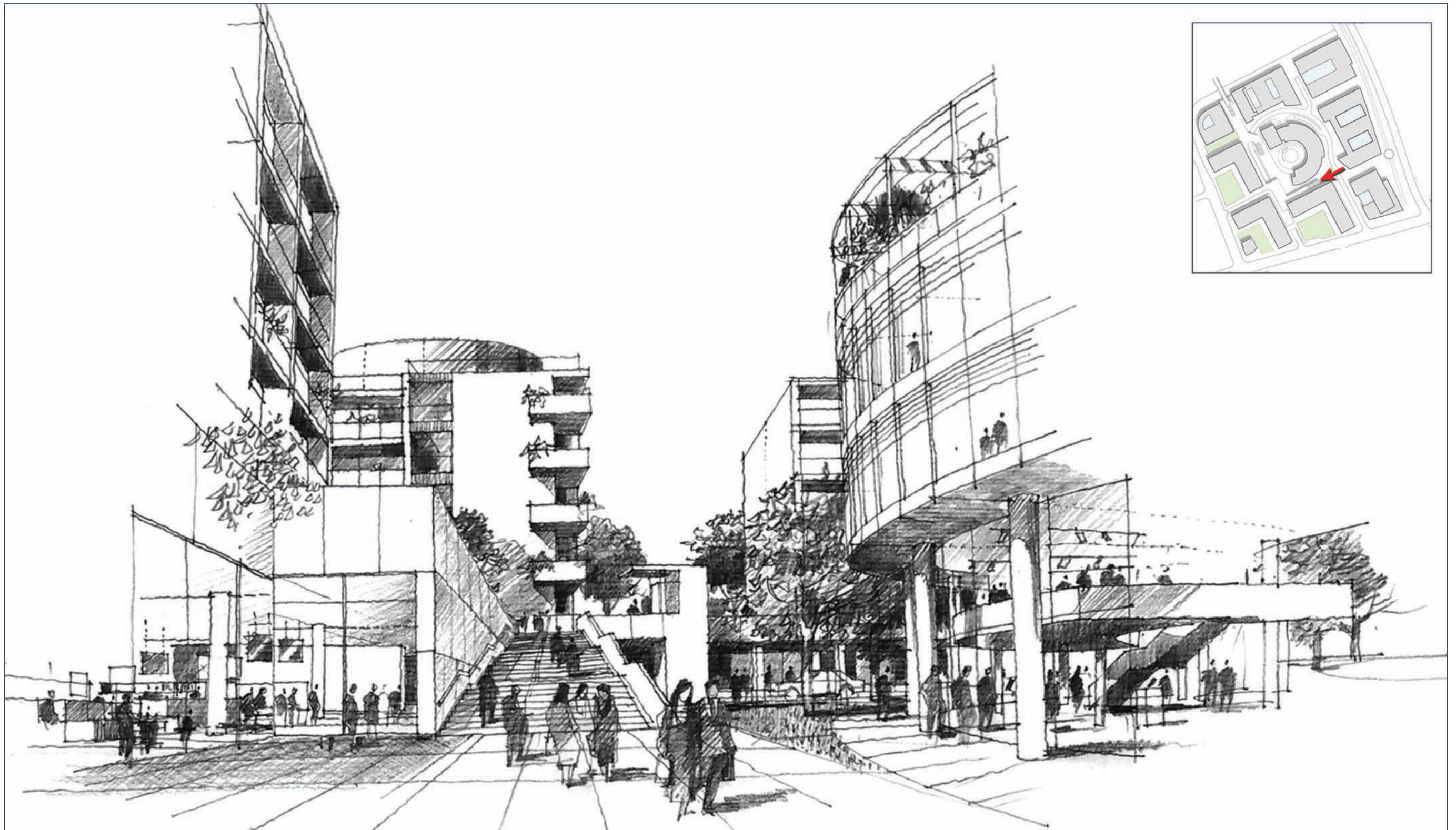


Figure 2: Illustrative view showing typical change of ground plane with steps and lift up towards the link over the Wyattville Link Road (block TC3 - 5) within TC3

3.2 Block Layout, Urban Grain & Design Approach

3.2.1 Block Layout

The block layout and urban grain is illustrated on Map 3 'Block Layout, New Ground Levels and Principal Frontages' in accordance with the requirement of Section 6.2 (b) of the Planning Scheme where it is required to include a

block layout for the Town Centre, illustrating urban blocks and urban grain and building typologies'. The design of any development in the Town Centre shall be consistent with this block layout, the street network and civic spaces. Flexibility in the design of the blocks and building form is allowed for during the detailed design stages, and at the discretion of the Planning Authority.



Map 3: Block Layout, New Ground Levels and Principal Frontages

The block layout defines a network of streets and public spaces between the blocks which form a hierarchy of civic spaces and a network of connections between them. These links are predicated on the principles identified in Section 2.10 and Map 2.5 'Access & Movement' of the Cherrywood Planning Scheme and the relevant requirements of Objective DA10 of the Planning Scheme as detailed below. The links shall provide a coherent network for pedestrians and cyclists to navigate and access public spaces and destinations.

Specific Objective DA 10 states 'The design and layout of the Town Centre shall address the following issues:

- In relation to ground levels, the change in levels between the surrounding road network and the Town Centre Link shall be designed in a manner that is attractive to the pedestrian;
- In relation to building heights, where a building addresses two streets, building height will be measured from the higher street.

- In relation to the Grand Parade, it is proposed as a low speed 30km/h street facilitating movement across the Wyattville Link Road for pedestrians, cyclists, Luas and motorised traffic. It will mitigate the severance caused by the Wyattville Link Road and encourage interaction between both sides of the Town Centre. In traffic terms it will provide vehicular access to development blocks TC2 and TC4 and reduce at-grade crossing movements of the Wyattville Link Road. Mitigation of the severance outlined, together with these access issues must be addressed.
- In relation to the relationship with the Wyattville Link Road, lands that encroach/abut/ or in any way have a relationship with the Wyattville Link Road, shall show the relationship between all building edges and the Road. The design of all elements shall be such that pedestrian movement is positively discouraged across the Link Road at-grade. This detail is necessary to support the creation of an attractive and sustainable environment in the Town Centre.'

The block layout creates a continuous building line along Grand Parade. This, combined with active frontage and the building height and scale, ensures that Grand Parade provides a sense of enclosure and fulfils its intended role as set out in the Planning Scheme as the main north-south spine through the Town Centre quadrants. For the remainder of the Town Centre, the block layout is made up of a number of perimeter blocks which defines a network of streets and public and private spaces.

Block numbers are included for ease of reference as shown in Figure 3.

3.2.2 Urban Grain

In terms of urban grain, the design approach for the Town Centre, seeks to achieve a fine grain at street level, in order to assist with the massing of the perimeter blocks and to create a human-scale. Frequent shop fronts and access creates a fine grain at ground floor level and in tandem with active uses, with a mix of retail, café, restaurants and non-retail uses, has the potential to animate the street and contribute to the vitality of the area.

There will be a focus on securing fine grain at key locations at the interface between high quality pedestrian routes, key civic spaces with sunny spill out spaces and a concentration of retail, retail services or other active uses. Innovative design solutions which present a fine grain at street level, contribute to streetscape qualities and respect the principles of good urban place-making, will be sought at detailed design stage.

There will also be a need to ensure that large-scale commercial developments incorporate design details which articulate individual elements so as to avoid monolithic façades and demonstrate careful detailing of the interface with the street to contribute to animation and streetscape quality.



Figure 3 Block Numbers within the Town Centre

3.2.3 Principal Frontages & Interface with the Wyattville Link Road

The block layout ensures that principal frontages are provided along Cherrywood Avenue, Tully Vale Road and Bishop Street, together with Grand Parade as illustrated on Map 3 and in accordance with Objective PD 9 of the Planning Scheme 'To provide principal frontages in each development plot to define strong street scape elements, turn corners on public roads and enclose and overlook amenity open space areas and green routes'. Building design shall ensure that these principal frontages are strengthened to fulfil this objective, in particular at the principal corners along the Wyattville Link Road, Bishop Street and Cherrywood Avenue. Together with the provision of active uses, in line with Section 2.2 'Active Uses and Frontages' and Map 2 and Section 3.2.1 Block Layout and 3.2.2 Urban Grain, defined, interesting and attractive building edges shall be provided. Particular regard shall also be had to those building edges which are not animated by use and activity, such

as along the Wyattville Link Road. In such cases attractive and interesting facades shall form part of the design. Figure 4 provides an illustrative example of how a two storey non-retail building (Building TC3-10) can define and animate the corner of Cherrywood Avenue. (See also Figure 7 Corner of Cherrywood Avenue and Grand Parade).

In accordance with Objective DA 10 of the Planning Scheme and as illustrated in Figure 5, the block layout creates a street network and provides linkages that encourage the movement of pedestrians through the Town Centre at the new ground level, avoiding the Wyattville Link Road and leading people towards Grand Parade and the Wyattville Link Plaza and bridge and the pedestrian/ cycle link between TC1 and TC3, both of which bridge the Wyattville Link Road as required by the Planning Scheme. The block layout also ensures that the buildings within all 4 no. quadrants do not front onto

or provide direct access along the Wyattville Link Road yet in the design of the buildings, an acceptable level of surveillance of cyclists shall be maintained. Landscaping along the Wyattville Link Road shall be in the form of a grassed central median and verge incorporating mature native street trees with understorey, woodland or hedgerow planting.



Figure 4: Illustrative view showing direct access from street level at south west corner of block TC3-10 at Cherrywood Avenue

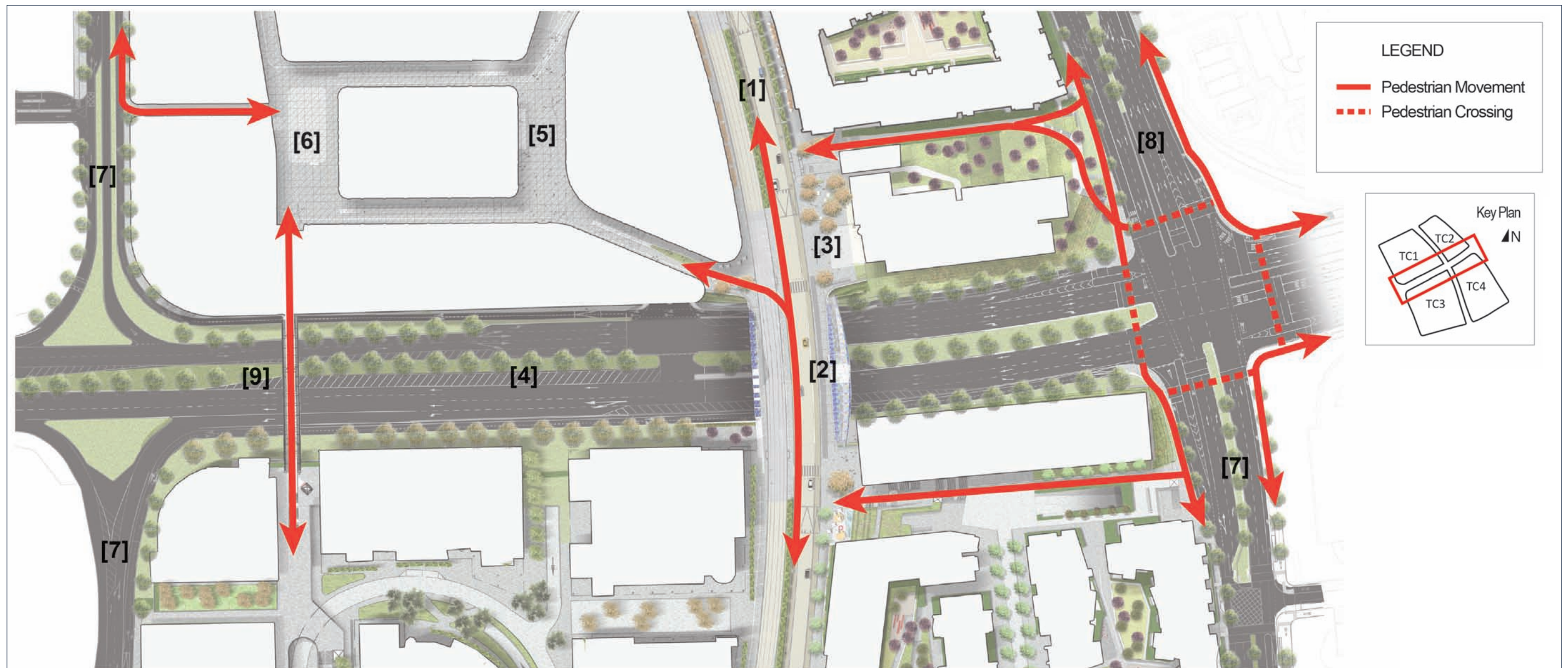


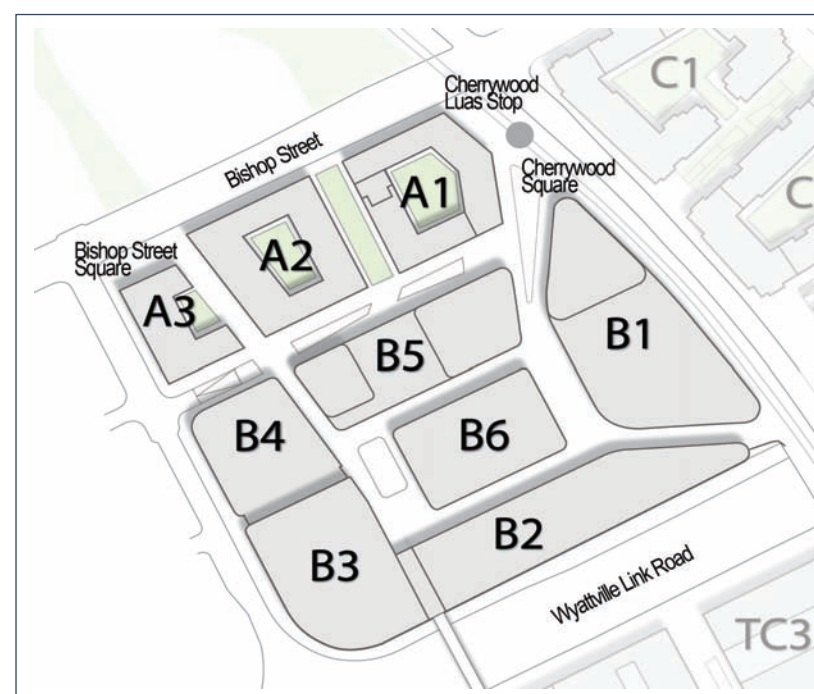
Figure 5: Interface with the Wyattville Link Road

3.2.4 Design Approach

TC1

From the main urban space and point of arrival at Cherrywood Square, defined as the principal focus and civic core, the block layout allows for a wide pedestrian street north of block B5, namely Civic Street. This street acts as a transitional zone between the residential and commercial uses provided within TC1. The principal community and civic uses, including a library and primary care centre, located within block B5, will be accessed from this street. North of 'Civic Street' the block layout defines three residential courtyard blocks referred to as A1, A2 and A3. These blocks front onto and provide activity along both 'Civic Street' and Bishop Street and on turning the corner of Cherrywood Avenue. The private spaces between blocks A1 and A2 shall provide ground level communal amenity open space and shall ensure own door access to create a street feel.

A clearly defined network of pedestrian and cycle streets is created by the block layout in TC1 which allows the pedestrian and cyclist to permeate throughout. From Cherrywood Square, pedestrians and cyclists will be directed towards the feature entrance to the principal focus and civic core between blocks B1 and B5. This will lead to a circular route of pedestrian streets within the area of



TC1 Quadrant

principal retail focus, revolving around the central block B6. These streets may be partially covered to screen from the weather however, it is essential that the street environment reads as an external space and the ground treatments and building elevation design shall use external materials and design references to a traditional street scape. At planning application stage it shall be demonstrated how this space shall remain an attractive pedestrian environment after trading hours.

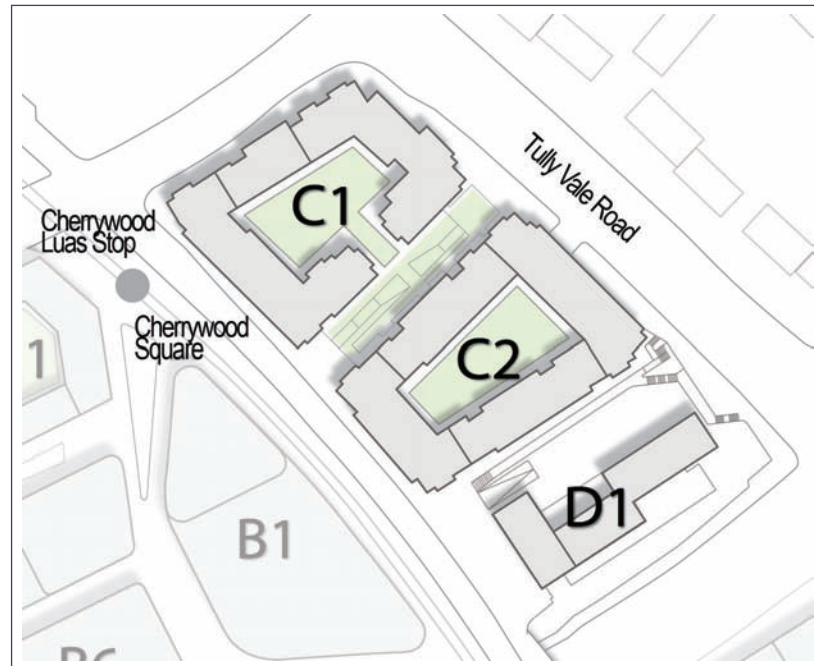
Commercial block B2 backs onto the Wyattville Link Road. This block should create interest along this route whilst restricting activity and access points to inhibit any desired crossing of the Wyattville Link Road by pedestrians. The design of blocks B3 and B4 will create a pedestrian link through to TC1 from Cherrywood Avenue to the west and will provide interest and activity when turning the corner of Cherrywood Avenue/Wyattville Link Road in accordance with Objective PD9 and Map 2.4 Principal Frontages of the Cherrywood Planning Scheme.

The block layout within TC1 opens up at points to create civic spaces, in particular between blocks B1 and B6, between blocks B3/B4 and B6 and also at Bishop Street to allow a physical and visual link to Tully Park to the north.

TC2

The block layout of TC2 includes three blocks C1 and C2 and D1. These blocks define routes east to west from Tully Vale and Junction A to Grand Parade.

The residential blocks C1 and C2, are arranged around central courtyards. The design of these blocks shall create a clearly distinctive residential environment, of an open and leafy nature. The position of these two blocks shall allow for quality private/communal amenity spaces between them. The orientation of the blocks and location of the private/communal amenity spaces shall be so as to optimise solar orientation and minimise overshadowing and overlooking. The western elevation of these blocks face Grand Parade and as such the ground floor will accommodate active uses to animate the street.



TC2 Quadrant

The third block D1, will accommodate non-retail uses and is ideally situated for hotel use and ancillary hotel uses (leisure, conference and other facilities). This building shall provide a strong presence to the important Wyattville Link Road Junction A. The block layout at this location and the building design shall ensure that the pedestrian is clearly directed away from the Wyattville Link Road to the routes through TC2 from Tully Vale to Grand Parade, as identified in The Pedestrian Movement Strategy and Public Realm Strategy included in the UFDF.

TC3

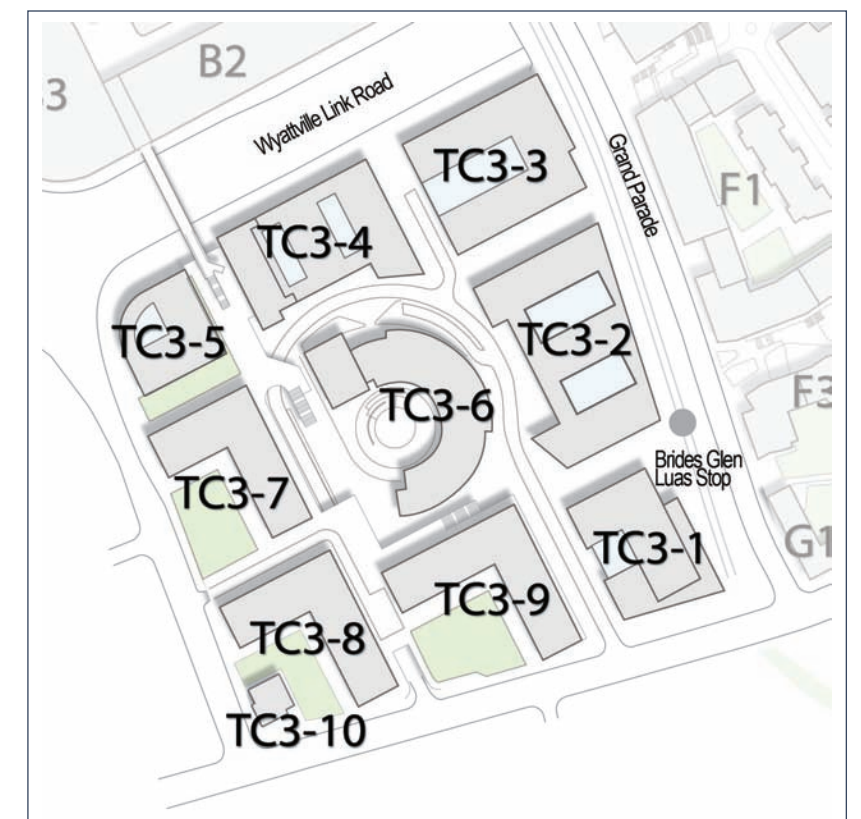


Figure 6: Illustrative View towards central civic space through block TC3-6

The block layout, block sizes and block footprints within TC3 are defined by principal and secondary pedestrian and cycle streets, including the link from TC4 and Bride's Glen Luas stop to Macnebury (Development Area 7 of the Planning Scheme) to the west, and the pedestrian/cycle link with TC1 across the Wyattville Link Road.

The block layout defines and frames the public realm with a series of human-scaled spaces designed to relate to the surrounding mix of uses and to ensure vibrancy and vitality throughout the day and in the evening (Figure 6 View towards central civic space TC3-6).

The block layout includes a series of buildings with floorplates and areas to provide flexibility for a mix of single and multiple lettings to meet the functional requirements for high intensity employment uses (HIE). Building TC3-6 is a central mixed use flexible building block which shall contain a combination of employment, non-retail, retail, retail services, community and residential uses. The building form is designed to create a sense of enclosure around the central civic space, whilst orientated to ensure adequate levels of sunlight penetrate this space. Links through this civic space are provided from the Bride's Glen Luas stop through to the pedestrian bridge over the Wyattville Link Road. The lower 'street' levels of buildings TC3-7, TC3-8 and TC3-9 include active uses such as retail, non-retail, and community uses and provide entrances into the more private communal residential areas, all of which will animate this frontage and pedestrian route.



TC3 Quadrant

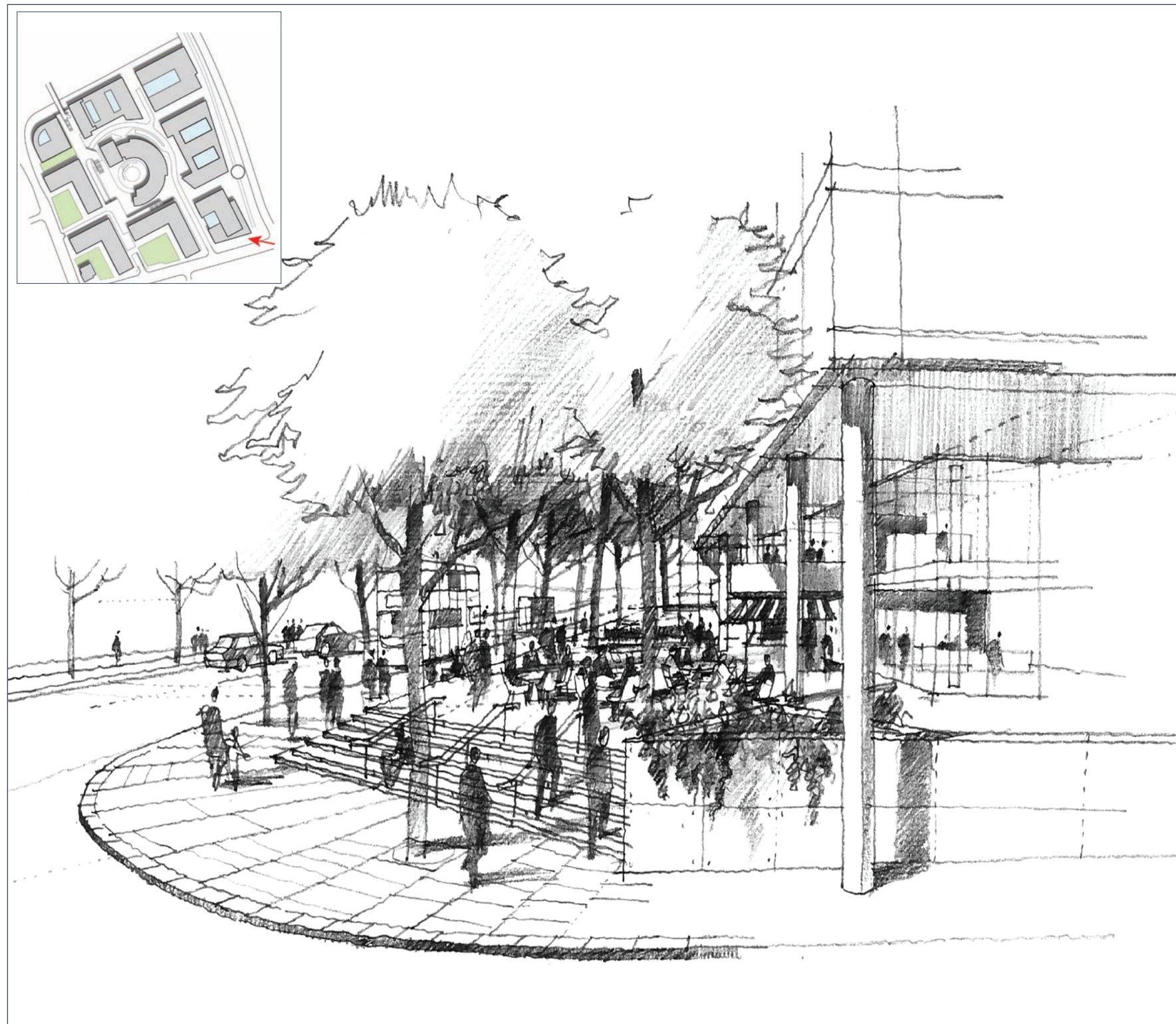


Figure 7: Illustrative view of block TC3-1 defining and animating the street corner at Cherrywood Avenue and Grand Parade

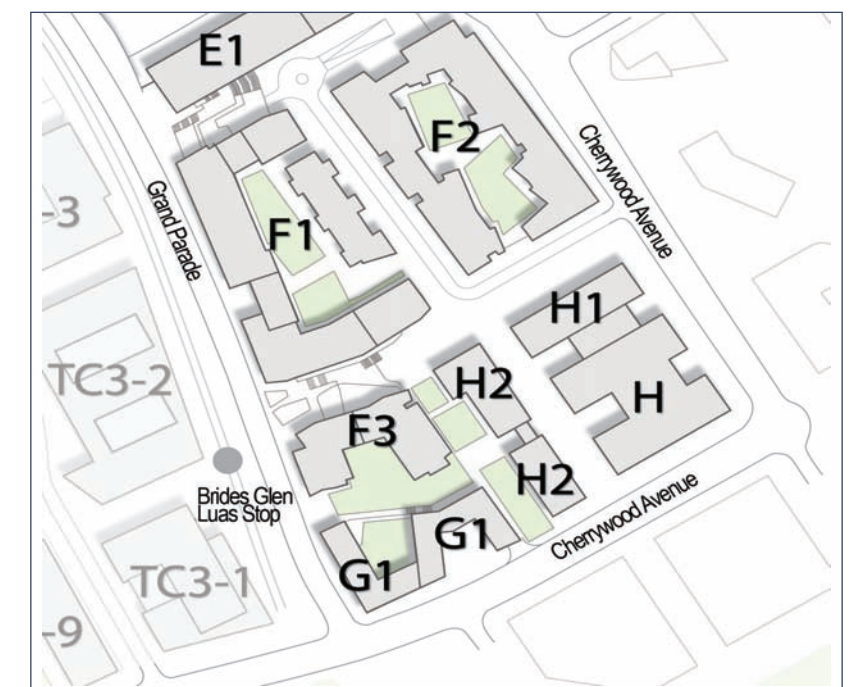
Above lower street level, buildings TC3-7, TC3-8 and TC3-9 mainly provide for residential apartment living. Given the mixed use nature of the entire quadrant, defined boundaries will protect residential amenity. Map 3 (Block Layout, New Ground Levels and Principal Frontages) proposes a defined streetscape along Cherrywood Avenue. Figure 7 illustrates how active uses at the ground floor to block TC3-1 can define and animate Cherrywood

Avenue at the corner of Grand Parade and demonstrates how the building at ground level may extend out onto a seating terrace before stepping down to street level, contributing to the overall urban streetscape (see also Section 2.1.1 'Active Uses & Frontages', Map 2 and Section 3.2.3 Principal Frontages, Map 3 and Figure 4).

TC4

TC4 has an internal at-grade street. This together with the provision of direct and attractive links from Cherrywood Business Park and Junction A, set up the block structure.

Block E1, located adjacent to the Wyattville Link Road Junction A is ideally suited to office use to buffer the residential blocks to its south. This building shall be of a quality design which creates a landmark at this prominent location, opposing the hotel use north of the Wyattville Link Road in quadrant TC2. The design shall ensure the street edge, on turning the corner at the Wyattville Link Road and Cherrywood Avenue, is clearly defined and that the ground floor addresses and animates this corner. The block layout and building design here shall be such as to direct the pedestrian and cyclist towards the routes identified on the Pedestrian Movement Strategy and Public Realm Strategy within the UFDF, through TC4 to Grand Parade, avoiding any conflict with the Wyattville Link Road.



TC4 Quadrant

The residential element within quadrant TC4 is broken into three blocks, F1, F2 and F3. The larger blocks (F1 and F2) are arranged as two large courtyard blocks located either side of the internal streets and a third block is located at the Bride's Glen Luas stop adjacent to Grand Parade. As within quadrant TC2, ground floor uses, fronting Grand Parade shall include uses to provide animation. As within quadrant TC2, the design of these blocks shall ensure a clearly distinctive residential environment is created. The position of these two blocks will allow for quality private/communal amenity spaces between them. The orientation of the blocks and location of the private/communal amenity spaces shall be so as to optimise solar orientation and minimise overshadowing and overlooking.

Block G1 is located in the area between the existing Office Block H and Grand Parade and is to comprise predominantly of non-retail uses. Uses which enliven the street and create a strong street scape shall be provided at ground floor level along Grand Parade and Cherrywood Avenue.

Block H is an existing office block with surface parking located on the south eastern section of TC4. Map 3 'Block Layout and New Ground Levels' indicates the potential for additional buildings and associated basement / undercroft car parking at this location.

3.2.5 Reconciling Ground Levels & Block Layout

The proposed ground levels across the Town Centre follow on logically from the approach to the establishment of new ground levels, responding to the land uses and the block layout. In accordance with the requirement of Section 6.2 (d) of the Planning Scheme where finished floor levels for buildings, including identification of entrance level(s), finished levels for roads and footways, and in particular, the horizontal and vertical connections between Grand Parade and the surrounding streets/buildings shall be demonstrated. In this regard, the street, ground, floor levels and entrance levels are illustrated in Map 4. The UFDF allows some variation to these as part of detailed design development to allow for practical requirements such as Building Regulations with particular regard to addressing universal access and drainage requirements in order to achieve an attractive user friendly and universal access public realm.

There is a 10-15 metre level difference to be addressed across the four Town Centre quadrants in an east-west direction and varying levels along the Luas line in a generally north-south direction. The fixed elements are the Luas line and the Wyattville Link Road and these levels are generally as follows:-

- Luas – From Cherrywood Square/ Bishop Street junction (circa 51.5m OD) Luas moves from over the proposed Wyattville Link Road bridge (circa 54.5m OD) sloping back down to circa 49m OD at Bride's Glen stop.
- Wyattville Link Road falls quite steeply from 56m OD at the junction with Cherrywood Avenue West to circa 46m OD beneath the Luas bridge and circa 42.2m OD at the Wyattville Link Road Junction A.

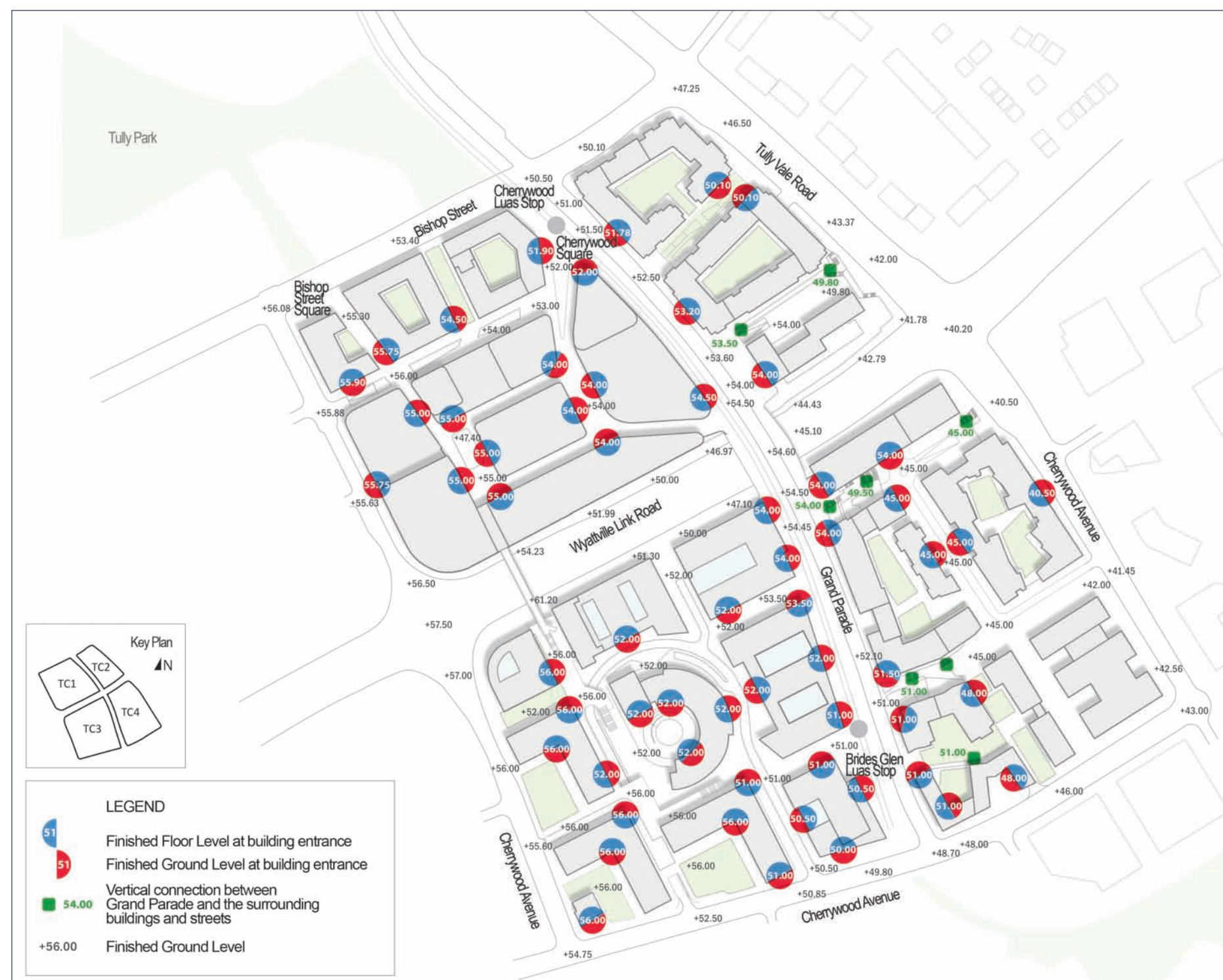
TC1

Cherrywood Square area is generally situated at circa 52m OD. The internal east-west pedestrian street (Civic Street) between the residential and commercial uses rises gently from circa 52m OD at Cherrywood Square to circa 56m OD at the road spur Wyattville Link Road - J. The ground level within the covered streets of the retail focus are generally set at circa 54-55m OD with gentle or imperceptible slopes. The Cherrywood Luas stop platforms are integrated with the ground plane of Cherrywood Square: the gradients and gentle falls of the Square are manipulated to ensure that the platforms link seamlessly with the level of the Square. The southern end of the platforms join in with Grand Parade's footpath levels as the level of the road rises towards the bridge. From Cherrywood Square, Grand Parade rises gently following the Luas line levels, from circa 52m OD to circa 54.5m OD at the apex on the Wyattville Link Road Bridge. The building frontages and internal floor levels will be stepped progressively and locally to ensure all entrances are at-grade with the footpath levels along Grand Parade.

TC2

TC2 will have a predominant podium level at circa 50m OD across the entire quadrant to meet with the adjacent streets. Crossing TC2, two internal east-west pedestrianised streets address the drop in levels between the Luas Line/Grand Parade to Tully Vale Road, one with communal access and one with public access. The publicly accessible pedestrianised street employs a combination of steps, ramps and other devices to negotiate this gradient change.

The east-west level difference across the quadrant varies from between approximately 5 metres from Bishop Street to approximately 11 metres close to the Wyattville Link Road. Ground floor levels within the edge blocks along Grand Parade follow the Luas levels generally and the set down area to the front of Block D1 on Grand Parade allows for the creation of a set-back and public space. On the other perimeter elevations to TC2, along Bishop Street, on the lower levels along Tully Vale Road and Wyattville Link Road careful consideration shall be given to achieving appropriate edge conditions at these locations and the buildings will be stepped down from the predominant podium level to meet existing road levels ensuring active frontages and surveillance of the perimeter streets.



Map 4: Finished Ground, Floor and Entrance Levels at Buildings

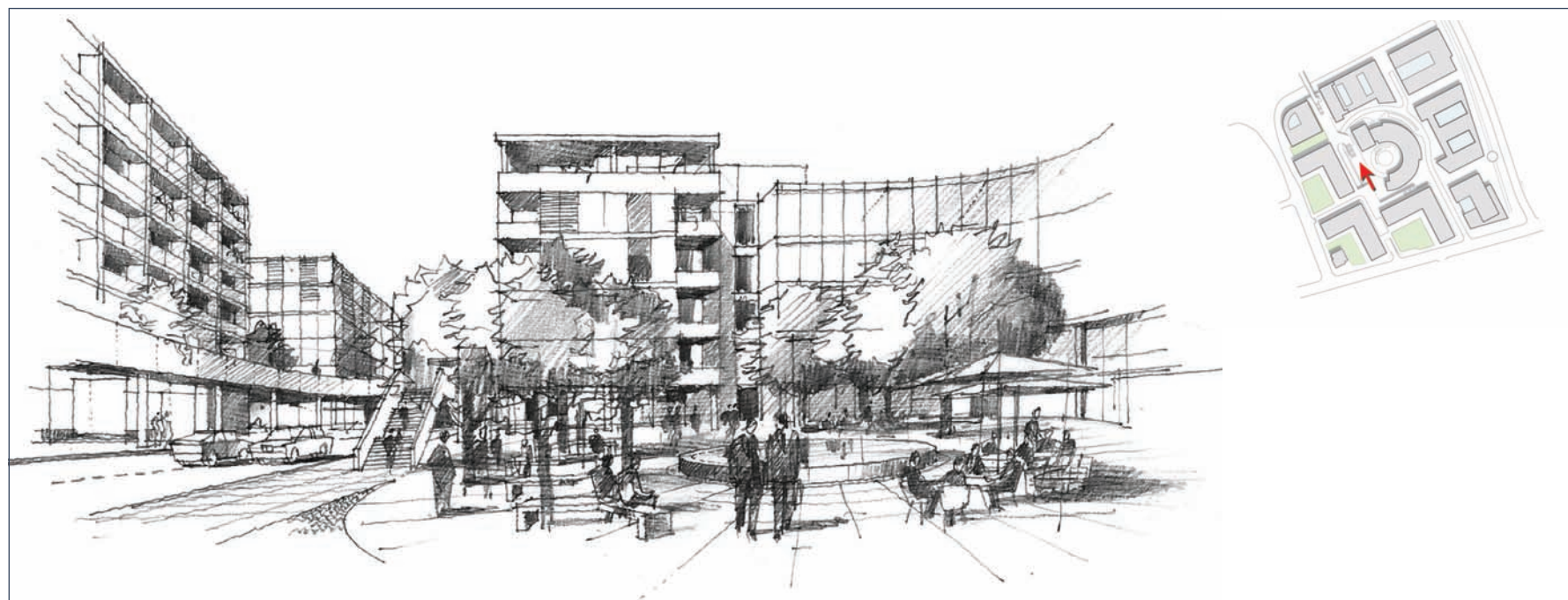


Figure 8a: Illustrative view across TC3 civic space towards steps to the link over the Wyattville Link Road to TC1

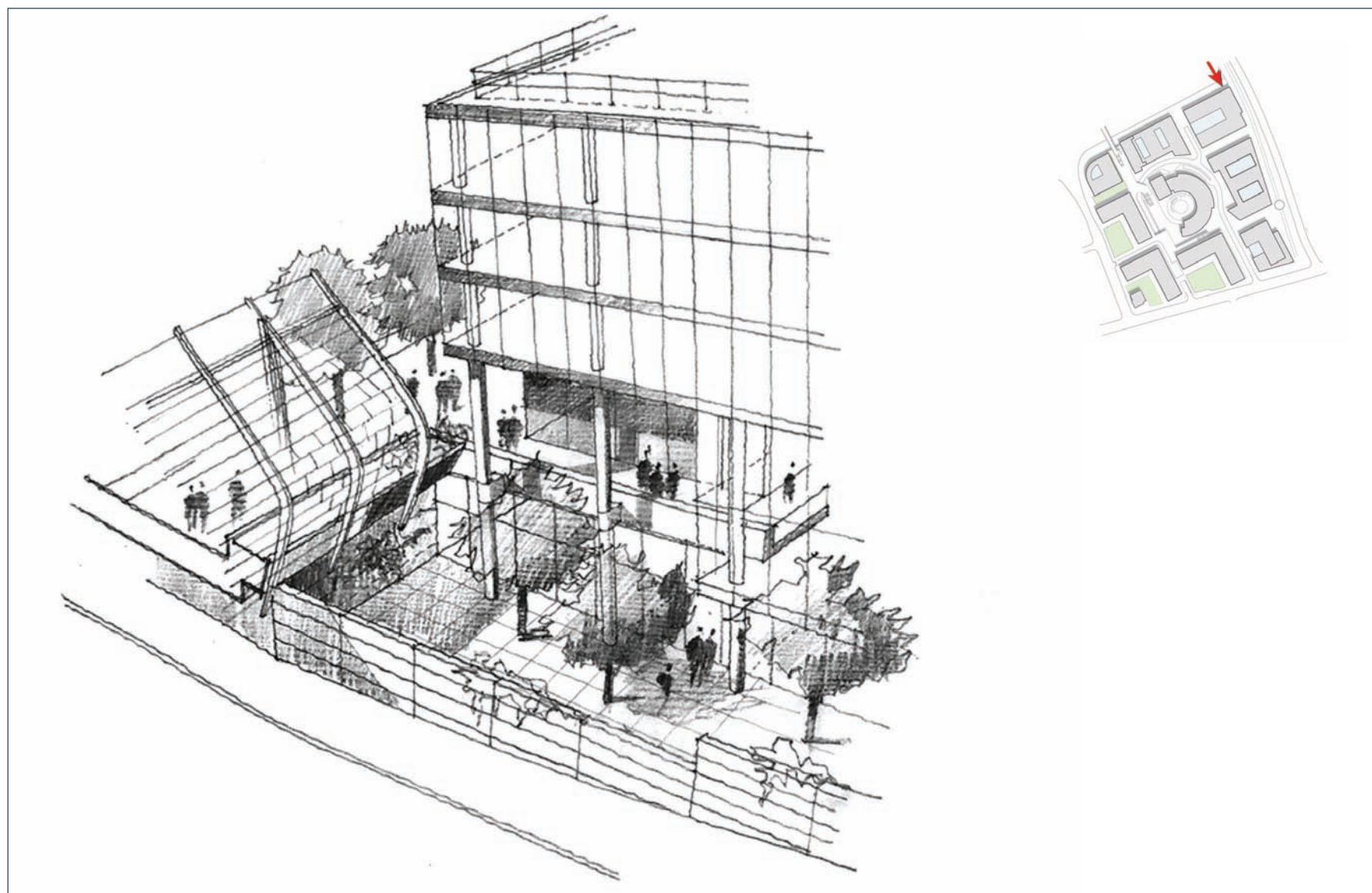


Figure 8b: Illustrative view showing level change adjacent to Wyattville Link Road and Grand Parade/Luas Crossing at block TC3-3

TC3

The existing ground levels in the vicinity of TC3 range from circa 49m OD at Cherrywood Avenue near the Bride's Glen Luas stop, to circa 55m OD at the junction of Cherrywood Avenue South and West, to circa 57m at the junction with the Wyattville Link Road. The levels along the northern boundary of TC3 with the Wyattville Link Road fall to circa 46m OD under the Luas crossing point. The proposed Grand Parade levels follow the gradient of the Luas line and rise gradually by approximately 5 metres from its junction with Cherrywood Avenue to the crossing of the Wyattville Link Road. The proposed two principal ground planes within TC3 will be circa 52m OD in the central and eastern part of the site, and circa 56 m OD towards the western and southern sides. There is a gentle slope from Grand Parade towards the centre of TC3 to meet level 52m OD. Steps with accessible and cyclist friendly lifts will be provided to bring people to the footbridge [circa 61.2m OD] which connects TC3 with TC1 across the Wyattville Link Road. This approach avoids excessively long and steep slopes that would be onerous and deter many pedestrians, cyclists, wheelchair users, etc [see figure 8a & Section 4.3.1]. One of the lifts provided shall provide direct access from ground plane circa 52m OD to the footbridge platform circa 61.2m OD.

The ground plane varies locally across the site blocks to deal with the varying ground levels. Generally building frontage will relate to the adjoining street level. In particular ground levels to blocks TC3-1, TC3-2 and TC3-3 relate to the adjoining street level along Grand Parade with gentle slopes to allow universal access and avoid the need for any external steps. This is achieved by stepping the ground floor level within the buildings. For example, the ground floor level for block TC3-1 will be circa 50m OD related to the ground level of Cherrywood Avenue/Grand Parade and circa 51.00m OD related to the primary pedestrian route into TC3 from Grand Parade at the Bride's Glen stop. This will be designed in detail for each building at the planning application stage.

The significant level difference of the Luas (circa 54.5m OD) spanning over the Wyattville Link Road (circa 46m OD) is addressed by providing the entrance level to block TC3-3 to meet the level of Grand Parade to create active frontages onto Grand Parade (Figure 8b) and by providing a lower level of accommodation at circa 48m OD, looking out onto the Wyattville Link Road providing natural surveillance.

TC4

TC4 changes significantly in cross-section from south to north across the quadrant towards the Wyattville Link Road. Along the eastern edge along Cherrywood Avenue, the levels drop from circa 43m OD at the south east corner of Cherrywood Avenue, to circa 40m OD at the junction of the Wyattville Link Road. Moving northwards at the first of two east-west internal streets, the ground level rises approximately 10 metres from Cherrywood Avenue (east) to Grand Parade at the Luas stop at Bride's Glen. At the second east-west internal street the levels rise approximately 14 metres from Cherrywood Avenue (east) to Grand Parade. Podium levels vary locally across the site to best negotiate the level differences. Similar measures to those employed on TC2 to deal with varying ground levels and ensure active frontages along the perimeter of the individual blocks will be required. The internal cross streets, predominantly pedestrian and cycle oriented routes, again use a combination of steps, ramps and other devices to address the significant differences.

Block E1, the HIE building fronting the Wyattville Link Road on the north edge of TC4, addresses the particular constraints of its site. It is split into two stepping volumes to accommodate the change in levels across the site along the Wyattville Link Road. It is arranged to provide an entrance façade to Grand Parade and steps down to address the Wyattville Link Road frontage and provide a second entrance at the lower end of the site at Cherrywood Avenue (east).

3.3 Building Height, Scale and Massing

The UDF is required to identify the height, scale, massing and building typologies within the Town Centre, as defined in Section 6.2 (c) of the Cherrywood Planning Scheme. The design of buildings, in terms of their height, scale and typology shall ensure quality accommodation and levels of amenity, in terms of acceptable levels of daylight and sunlight provision. In this regard, the development shall be guided by the principles of "Site layout planning for daylight and sunlight: a guide to good practice, (2011, BRE Document BR 209) in conjunction with "Sustainable Urban Housing: Design Standards for New Apartments, Guidelines for Planning Authorities", DECLG, 2015 and "Guidelines for Planning Authorities on Sustainable Residential Development in Urban Areas (Cities, Towns and Villages)", DEHLG, 2009. A shadow, daylight and sunlight assessment will be required at planning application stage. Any development shall also be assessed having regard to Section 4.7 of the UDF which relates to micro climate for amenity space.

3.3.1 Scale and Massing

The built form to accommodate the quantum of floorspace and land use allocations within the Town Centre is governed by the minimum and maximum plot ratios as set out in Section 2.6 of the Cherrywood Planning Scheme which aim to achieve appropriate levels of sustainable development. Based on the future form as illustrated in the UDF, the plot ratios and site coverage are as shown in Table 2 below. The design of the Town Centre shall demonstrate that it accords with the plot ratio and site coverage range.

Table 2: Plot Ratio & Site Coverage				
Town Centre				
The site coverage (min-max) range is 50% to 80%				
The plot ratio (min-max) range is 1.8 - 2.3				
	TC1	TC2	TC3	TC4
Plot Ratio	2.2	2.2	2.25	2.2
Site Coverage	72%	57%	54%	59%

3.3.2 Building Height

The building height (min-max) range within the Town Centre is between 2 & 5 storeys. In recognising the conditions which will arise from building on different levels, Objective DA10 of the Planning Scheme states that, where a building addresses two streets, building height will be measured from the higher street which means that parts of a building can exceed 5 storeys to address ground levels.

Map 5 'Building Height' and the Section drawings (Figures 9, 10 & 11) illustrate the scale, massing and building heights of the blocks for the 4 no. Town Centre quadrants.

The design of the Town Centre in terms of scale, massing and building height, shall seek to achieve the density and critical mass which will create an appropriate urban character for the Town Centre. It shall also ensure to create a local environment that is not negatively impacted on by the micro climate created, in particular by creating wind tunnels or excessive shade.



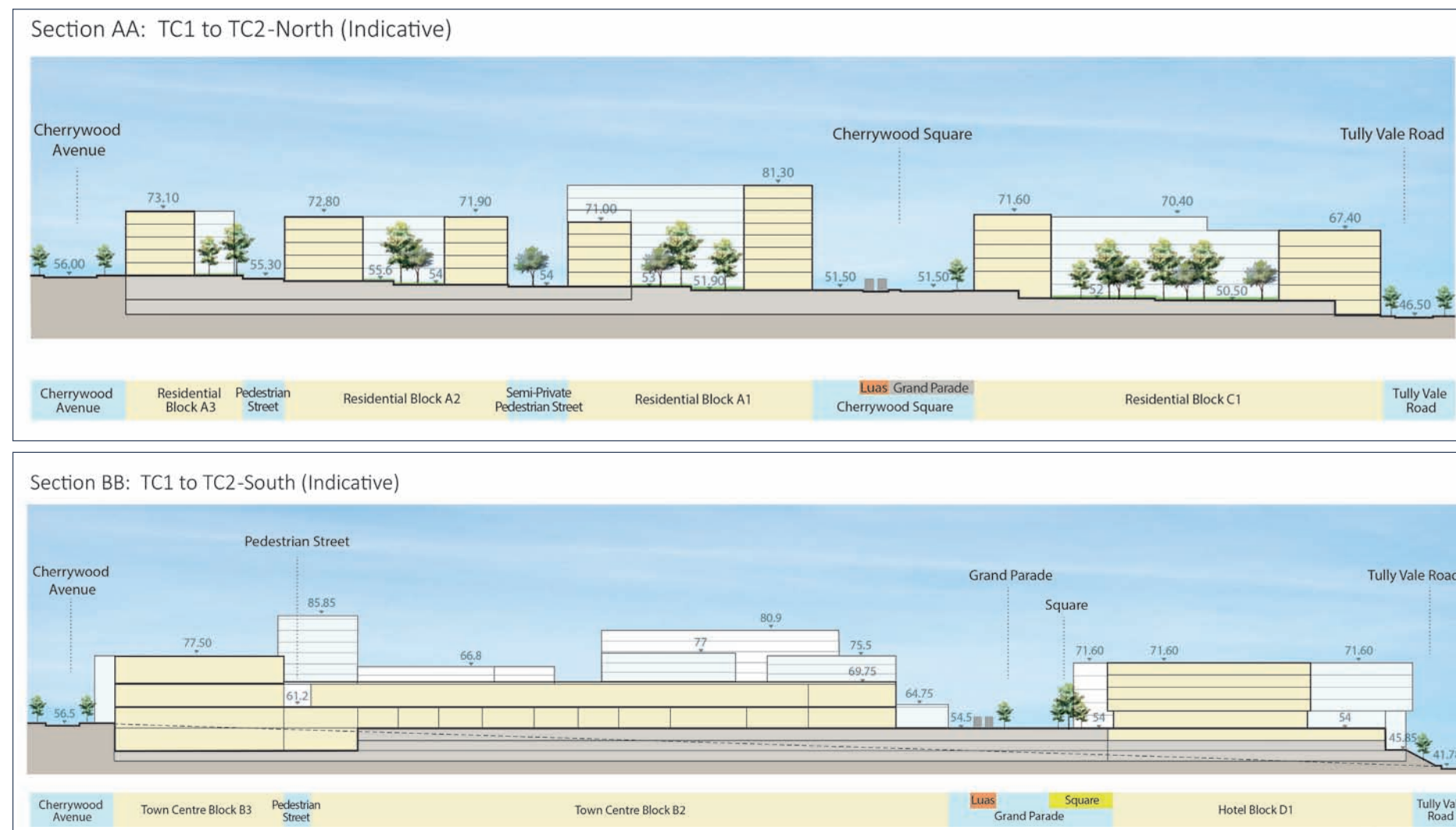


Figure 9: Sections AA and BB, TC1 to TC2

3.3.3 Upward Modifiers

The application of upward modifiers are provided for at 4 no. locations across TC1, TC3 and TC4 and identified on Map 5 Building Height. Upward modifiers allow a local increase in height of up to 3 storeys. The urban design benefits comprise improved legibility, more diversity in roof profile and enhanced character in the appearance of an urban development. The permitted locations are limited and are selected on a number of criteria: proximity to major public transport infrastructure; addressing and enclosing civic spaces; impact on neighbouring buildings; and civic or cultural importance of the development. The UDFD identifies these locations whereby the urban design benefits are best realised within the Town Centre. In some circumstances these locations differ from those illustrated on Map 2.3 of the Planning Scheme but are considered not to materially alter Objective PD22 of the Planning Scheme and are considered to best realise the overall objective as defined below:

PD22: Local landmark and feature elements over the stated building heights are acceptable at important locations where they contribute to the visual amenity, civic importance and legibility of the area'

Upward modifiers are illustrated on Map 5 and as detailed below.

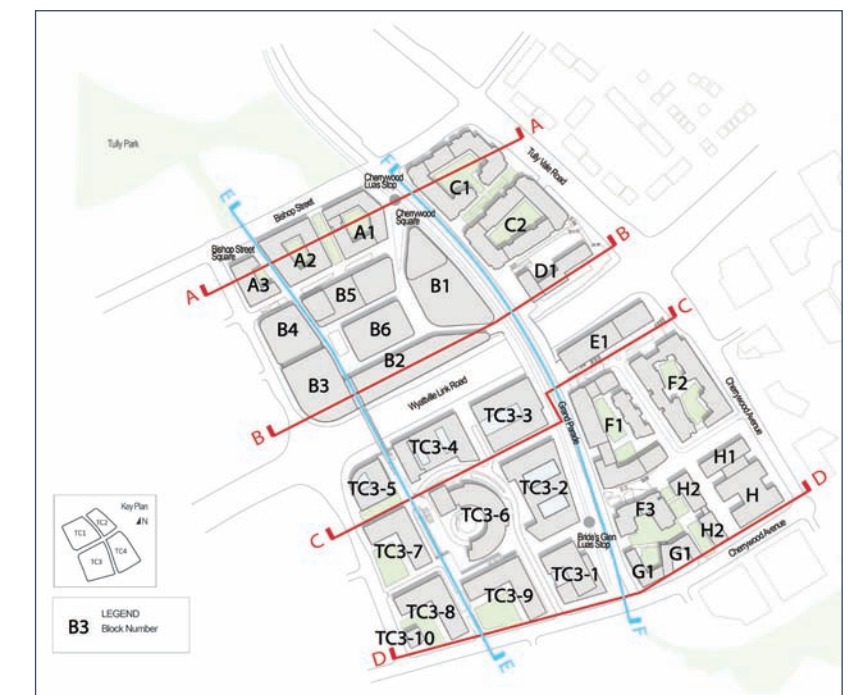
TC1

The Cherrywood Square area is the most appropriate location for the application of higher building elements as it marks the point of arrival and introduction to the Town Centre. In this case, the use of an upward modifier is important to improve the legibility of the urban form, to differentiate the space from others by virtue of an increase in building height and by providing a more prominent emphasis in the hierarchy and legibility of the urban form.

In Cherrywood Square, the residential building A1, facing the Square to the east, is assigned the upward modification in building height to combine with the two adjoining 5 no. storey retail/commercial office buildings in block B1 to form a coherent front or enclosure to Cherrywood Square and signify the main entrance into Cherrywood Town Centre. The space itself needs to be considered as a unified element, conceived as an urban set piece, and that the building heights and massing that define and enclose this space, shall be complementary in scale and appearance. Particular attention shall be given to elevation treatment to create interest and instil human scale.

The upward modifier, in this case, to the residential block A1, is continued around the corner onto Bishop Street. This is an important urban design element, firstly to consolidate the enclosure of Cherrywood Square but also to address and frame the area which extends towards the entrance to Tully Park. The resultant building is eight storeys in height on two flanks and of an appropriate scale and physical presence to address these spaces and an important approach to the Town Centre. Block A1 steps down in height to the south and west corners to engage with the lower scaled streets on those sides.

The Planning Scheme permits a second upward modifier in TC1. The location of this will act as a complementary local landmark and entrance into the North West of Town Centre from Tully Park. A residential element of six storeys sits on the west part of block B5, a two to three storey commercial building, to form an eight storey tower. This residential tower is located at the intersection of 'Civic Street' and the diagonal street lying on a desire line leading up to Bishop Street Square and ultimately Tully Park located on the hill above the Town Centre to the north. The function of this higher building is to signal the Town Centre location when viewed from a distance and especially from the higher ground of the park and residential developments to the north-west and to mark this important pedestrian and cycle route into the Town Centre.



The application of these upward modifiers, in the locations described above, utilising the 2 no. upward modifiers designated for TC1, delivers on the purpose intended and greatly contributes to realising the urban design objectives of the Planning Scheme and improving the ultimate urban form of the Town Centre.

TC2

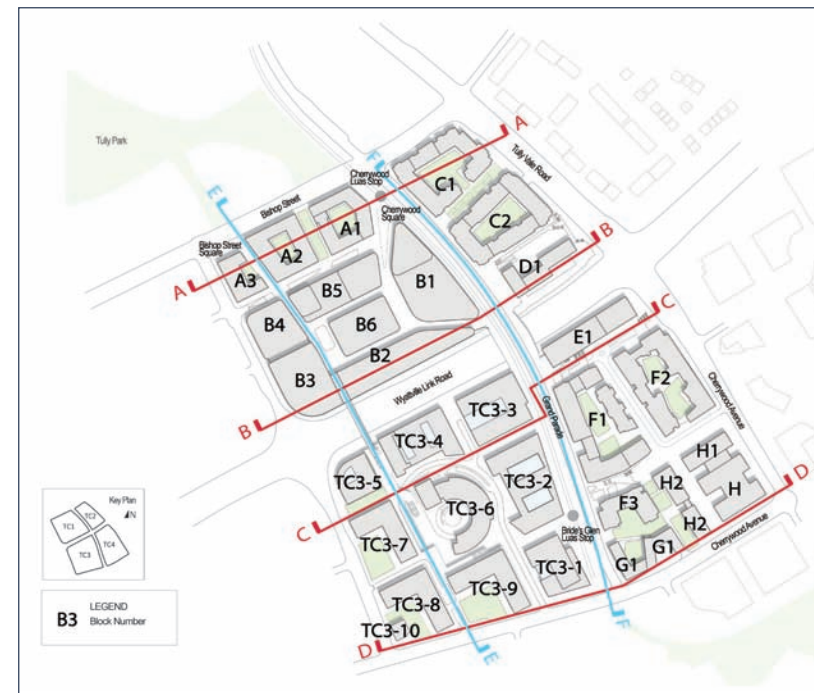
There is no proposal for an upward modifier on TC2.

TC3

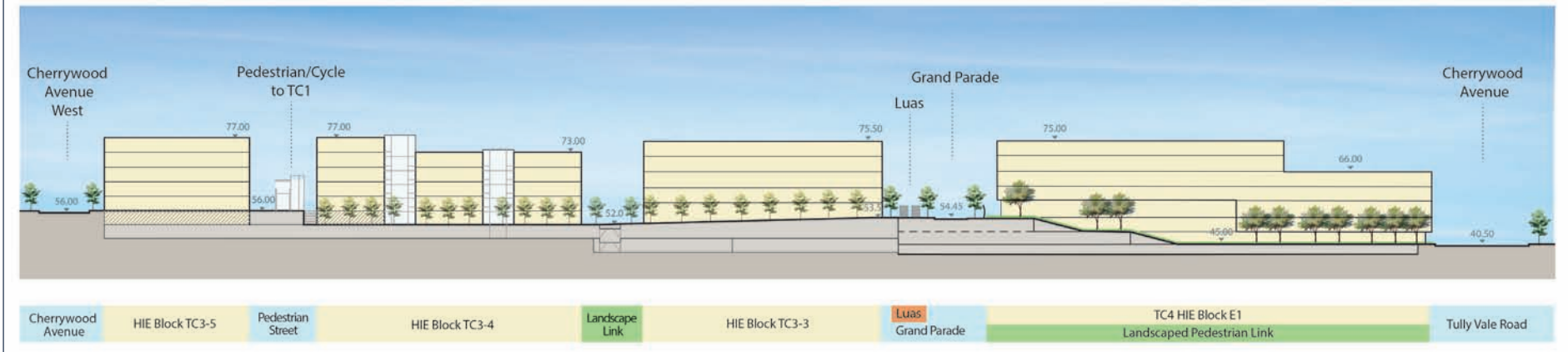
The purpose of upward modifier at block TC3-1 is to mark the location of the Bride's Glen Luas stop, its public function and to address the civic space to be created around the stop. The building height here will provide a visual landmark when viewed from street level from Grand Parade to the north, Cherrywood Avenue to the west and east and from within TC3.

TC4

The upward modifier within TC4 is also located at Bride's Glen Square, opposing that in TC3. Block F1 increases in height to eight storeys on its south-west corner (and 9 storeys where it addresses the lower street) to mark the location of the Luas stop and civic space and at the culmination of one of the east-west internal streets through TC4. The building steps down to 5 no. storeys for the remainder of the perimeter block enclosing a landscaped courtyard within.



Section CC: TC3 to TC4-North (Indicative)



Section DD: TC3 to TC4-South (Indicative)

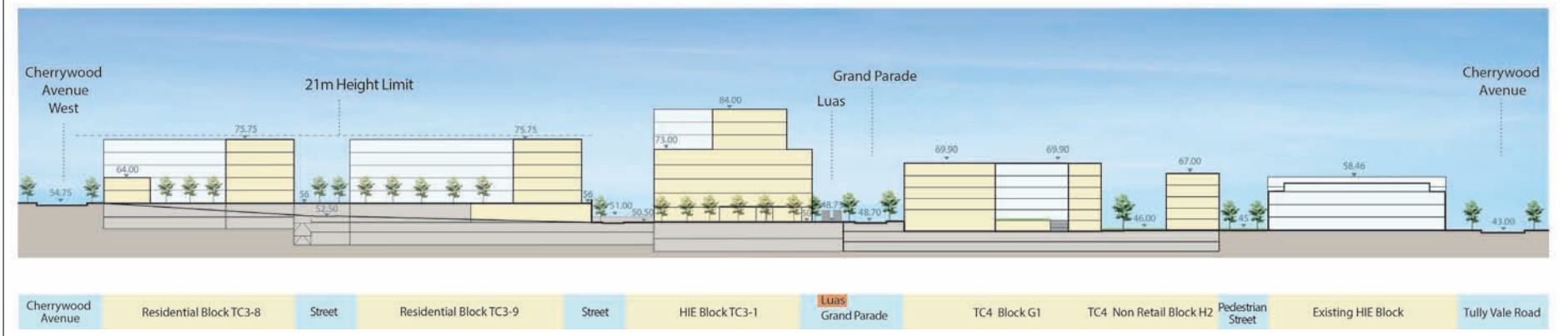
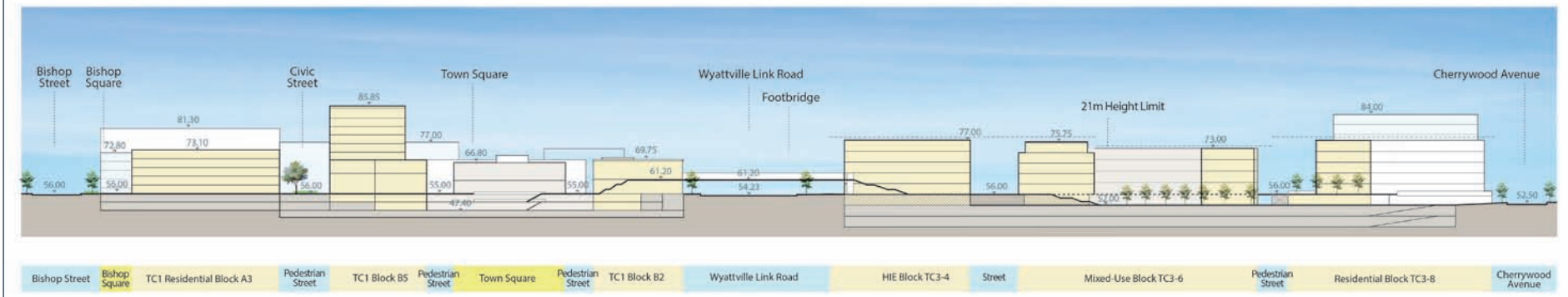


Figure 10: Sections CC and DD, TC3 to TC4

Section EE- TC1 to TC3 (Indicative)



Section FF- Grand Parade Towards TC2 and TC4 (Indicative)

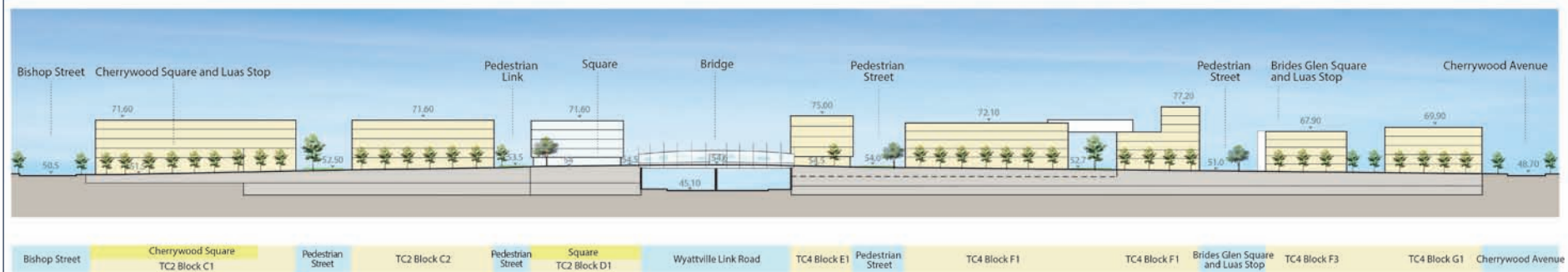


Figure 11: Sections EE and FF across the Wyattville Link

3.4 Establishing Quality

In accordance with the requirements of 6.2 (e) of the Planning Scheme ‘to address the location, design and treatment of squares, civic spaces, pocket parks, and the public realm generally’, this section demonstrates the level of quality of materials required throughout the Town Centre development. Quality manifests itself through the layout of the urban form, building design and through the quality of materials and finishes all contributing to the appearance of the environment. In this regard the design of the Town Centre shall adhere to the principles set out in Section 2.8 ‘Urban Form’ of the Cherrywood Planning Scheme.

In general, and in accordance with Objective PD14 of the Planning Scheme, the built environment within the Town Centre shall be so to ensure that ‘the distinctiveness of materials is used at various scales, allowing for a coherent and high quality built environment....high quality materials are to be used in the public realm, including external elevations of buildings, structures and public open space’.

Cherrywood Town Centre shall be designed to provide a high quality, legible and inclusive environment, providing for different groups in different ways. It shall provide for ease of access around the Town Centre and to public facilities for use by all. The appearance of buildings in their design and finish, together with the block layout, shall ensure to avoid visual or physical barriers and to create a clear distinction between public, communal and private areas. Buildings shall provide active frontages to create vitality where appropriate and their design shall use a palette of high quality finishes to ensure facades are interesting and attractive. Furthermore, any location of plant and services on roof tops shall be designed to mitigate visual impact. Any design shall accord also with Section 2.2 Active Frontages and Section 3.0 Urban and Built Form.

In their design, buildings shall make a positive contribution to the environment and engender ‘civic pride’. General principles which will be adhered to are outlined below and illustrated by means of reference images and a representative palette of materials (Figure 12 & 13). Note should also be given to Section 4.0 ‘Public Realm Strategy’ which further develops the

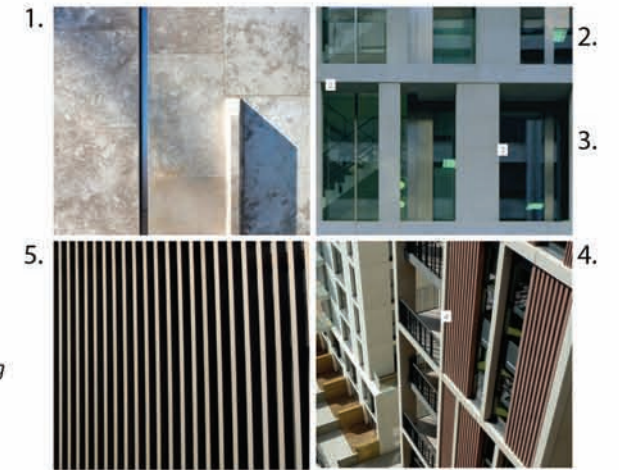
concepts around the design of the civic spaces, streets and public realm generally. In principle, the building and surface materials throughout the Town Centre shall:

- be of high quality, durable and attractive
- be practical to source, erect, replace, and maintain
- demonstrate environmental performance
- be compliant with all building regulation standards and requirements which may change during implementation of the Planning Scheme
- meet the user specification and standards, including those relating to environmental sustainability and wellness.

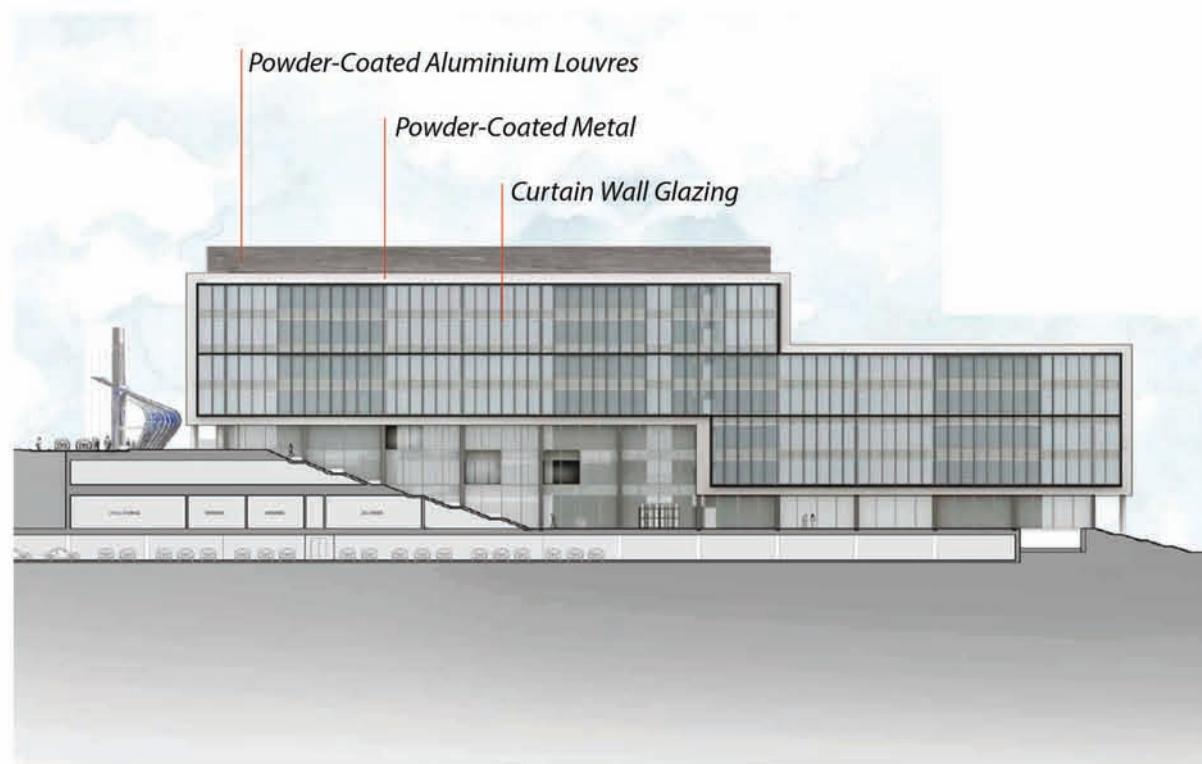
Given the relatively small area of the Town Centre, consistency in design and elevation treatment will be sought across the range of buildings proposed. The aim is to create a coherent townscape rather than a collection of standalone designs. In this regard a small palette of materials will be sought and simplicity in elevation treatment (particularly along the Grand Parade) will be required on the public and principal elevations of buildings.



Figure 12: Material Proposals Residential Development (Illustrative only)



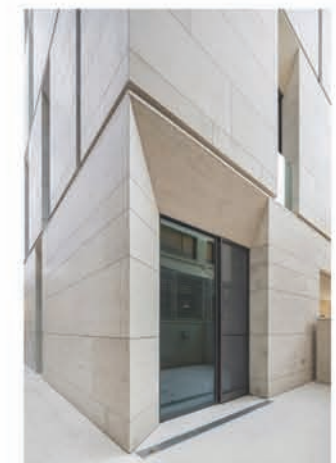
1. Natural/Reconstituted Stone Cladding
2. Bronze coloured finish window frame
3. Large scale windows with deep reveals
4. Bronze coloured finish cladding
5. Bronze coloured finish louvres



HIE Building TC4



Bronze coloured aluminium



Splayed light coloured natural stone cladding



Hotel TC2

Figure 13: Material Proposals Non Residential Development (Illustrative only)

Chapter 4: Public Realm Strategy

4 Public Realm Strategy

This Section details the Public Realm Strategy for the Town Centre required by Sections 6.2 (e) and 6.2 (g) of the Planning Scheme to address 'the location, design and treatment of squares, civic spaces, pocket parks, the public realm generally' and 'the treatment of Grand Parade'. A Public Realm Strategy is paramount to the establishment of a 'place of quality'. This Strategy draws together the block layout and addresses the existing and new ground levels; the movement of people; as well as the quality and usability of the spaces and connections within the Town Centre.

The Public Realm Strategy has been prepared in accordance with a number of specific objectives of the Planning Scheme, in particular DA13, G11 and PD20 below.

Objective DA13: 'To design civic spaces to assist movement through the Town Centre, in particular marking where pedestrian routes converge or cross',

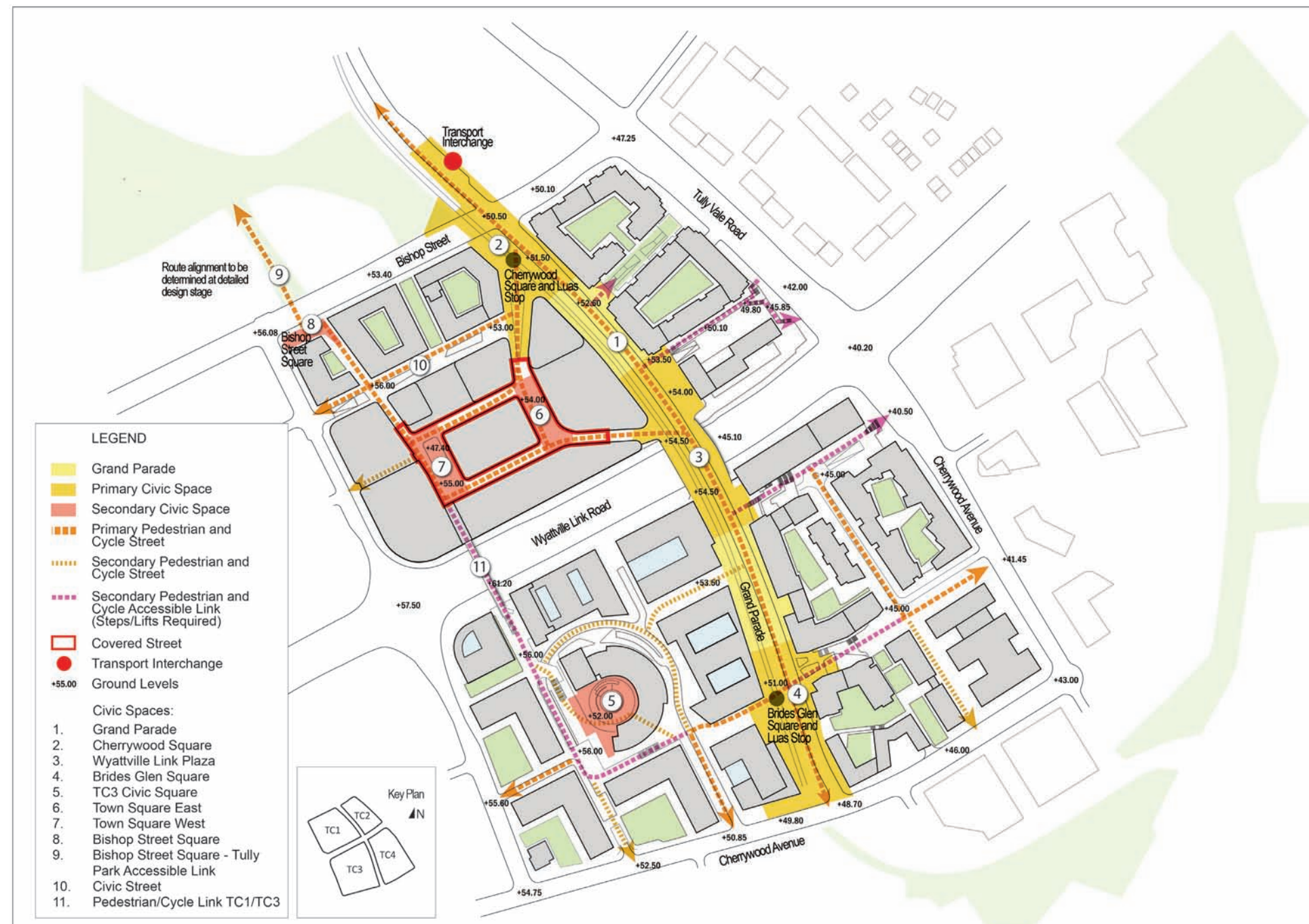
Objective G11 of the Planning Scheme states: 'It is an objective to require the provision of Civic Space within Cherrywood Town Centre to provide a civic setting and cater for pedestrians and the employment population. This provision shall be in line with the current County Development Plan qualitative and quantitative requirements and will be set out in the preparation of the overall Urban Form Development Framework for the Cherrywood Town Centre.'

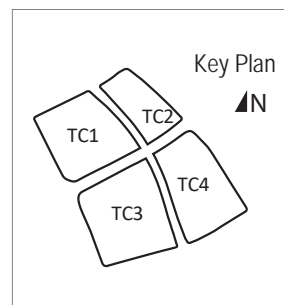
Objective PD20 'civic spaces will be fully accessible to all users, have a legible layout with clearly defined desire line routes and be composed of high quality /durable materials with a SuDs function that shall have a good mix of hard and soft landscaping elements.'

The Strategy identifies a hierarchy of civic spaces as identified on Map 6, Map 7 and Figure 14a-c and as follows:

- 1) Grand Parade
- 2) Cherrywood Square, Tully Park Link & Transport Interchange
- 3) Wyattville Link Plaza & Bridge
- 4) Brides Glen Square
- 5) TC3 Civic Square
- 6) Town Square East
- 7) Town Square West
- 8) Bishop Street Square
- 9) Bishop Street/Tully Park Link
- 10) Civic Street
- 11) Pedestrian/Cycle link TC1-TC3

Map 6: Public Realm Strategy





LEGEND

- [1] Grand Parade
- [2] Cherrywood Square
- [3] Wyattville Link Plaza
- [4] Bride's Glen Square
- [5] TC3 Civic Square
- [6] Town Square East
- [7] Town Square West
- [8] Bishop Street Square
- [9] Bishop Street / Tully Park Accessible Link
(Route alignment to be determined at detailed design stage)
- [10] Transport Interchange
- [11] Pedestrian and Cycle link TC1 - TC3
- [12] Hotel Forecourt

Map 7: Landscape Master Plan

4.1 Grand Parade

Grand Parade (No.1 - Map 6)



Figure 14a: Grand Parade Concept Design

Section 6.2 (g) of the Planning Scheme requires the UDFD to address: 'the treatment of the Grand Parade generally, including the proposed approach to the crossing of the Wyattville Link Road, demonstrating how an attractive and welcoming urban environment can be maintained, having regard to its important transport function as set out in the Planning Scheme.'

Objective DA10 of the Planning Scheme states 'In relation to the Grand Parade it is proposed as a low speed 30km/h street facilitating movement across the Wyattville Link Road for pedestrians, cyclists, Luas and motorised traffic. It will mitigate the severance caused by the Wyattville Link Road and encourage interaction between both sides of the Town Centre. In traffic terms it will provide vehicular access to development blocks TC2 and TC4 and will reduce at-grade crossing movements of the Wyattville Link Road. Mitigation of the severance outlined, together with these access issues must be addressed'.

Objective DA10 further states that 'In relation to the relationship with the Wyattville Link Road, lands that encroach / abut / or in any way have a relationship with the Wyattville Link Road, shall show the relationship between all building edges and the Road. The design of all elements shall be such that pedestrian movement is positively discouraged across the Link Road at-grade. The detail is necessary to support the creation of an attractive and sustainable environment in the Town Centre'.

The following sets out detailed design criteria for Grand Parade, Wyattville Link Plaza and the other civic spaces along Grand Parade in accordance with the requirements of the Planning Scheme.

Within the Town Centre, Grand Parade extends from Cherrywood Luas stop to Bride's Glen Luas stop, a distance of circa 530 metres. Grand Parade is the spine of the Town Centre, with a number of civic spaces located along it connecting a north-south network of Primary and Secondary Civic Spaces (Figure 14a-c).

Along Grand Parade, Cherrywood Square (No. 2 Map 6 and Figures 14b and 17) forms the principal urban space, commercial heart and civic focus of the Town Centre.

Bride's Glen Square (No. 4 Map 6 and Figures 14b and 22) is another major urban space centred on Bride's Glen Luas stop which is the main point of arrival for a significant resident and working population and it is envisaged that this space is likely to become the 'entrance' to Cherrywood from the south on extension of the Luas / Metro to Bray.

Cherrywood Square and Bride's Glen Square, as the two principal spaces are connected by the Wyattville Link Plaza (No. 3 Map 6 and Figures 14b and 19). This plaza is a significant bridge structure which forms a critical north-south link, enabling seamless movement and connectivity for all modes of transport across the Town Centre at the new elevated ground level.

The block layout or buildings along Grand Parade shall provide appropriate levels of setback to allow for the creation of secondary civic spaces, such as a wider footpath widths or pocket squares. In particular, such setbacks shall be provided at the Bride's Glen Luas stop and both ends of the Wyattville Link Plaza. Views shall be provided from Wyattville Link Plaza towards Dublin Bay to the East and the mountains to the West. Landscape and neighbourhood views shall be provided for within the Primary and Secondary Squares located along Grand Parade.



Figure 14b: Grand Parade: Network of Primary Civic Spaces



Figure 14c: Grand Parade: Network of Secondary Civic Spaces

The proposed tree structure of free-growing and pleached specimen trees will form a strong curved tree lined avenue and punctuate destinations in the form of plazas and civic spaces. Stretches of ground level linear planting within the central median will provide both guidance to the user to pre-determined crossing points, so as to mitigate interaction with cyclists, vehicles or the Luas and also introduce a softer element to the overall design. Both hard and soft landscaping palettes define Grand Parade as an 'urban' space with priority to pedestrians and cyclists. The Access and Movement Strategy for the Town Centre has been devised to reduce the requirement for larger vehicles or service traffic to use this route for access, an essential pre-requisite to achieve the high level of environmental quality and attractive character intended for Grand Parade.

Street furniture, lighting and service poles shall be carefully integrated within the Grand Parade landscape design and where possible shall be located on buildings to reduce clutter. Other creative solutions for play and street

furniture will be incorporated along the space, for example public art (identified by red and white star symbol Map 7), sculptural play elements and integrated seating and cycle stands within water features, planters and walls.

A typical section of the Grand Parade and typical landscaping plan are illustrated below (Figure 16 (a) – (g)). The section and plan drawings convey the quality of the spaces to be achieved through a palette of high quality materials. The palette of paving materials including natural and artificial stone, asphalt surfacing with and without chippings and surface treatments all utilising a variety of textures, patterns and tones, will help delineate spaces and define clearly a legible public realm. The high quality of materials and finishes throughout the Town Centre are to extend to Grand Parade and the Luas track surface, with appropriate alternatives to be provided in lieu of the existing track surface to achieve a quality Town Centre environment. In this regard, particular attention must be given to creating a seamless quality finish across the section of Grand Parade, delineating areas with the use of

paving materials through a high standard of design detail in paving size, colour, texture and finish. In so far as practicable, the levels of Grand Parade across its entire width should be flush, avoiding the use of kerbs and steps where feasible, so as to prioritise the crossing of pedestrians (Figure 15).



Figure 15: Illustrative view along Grand Parade

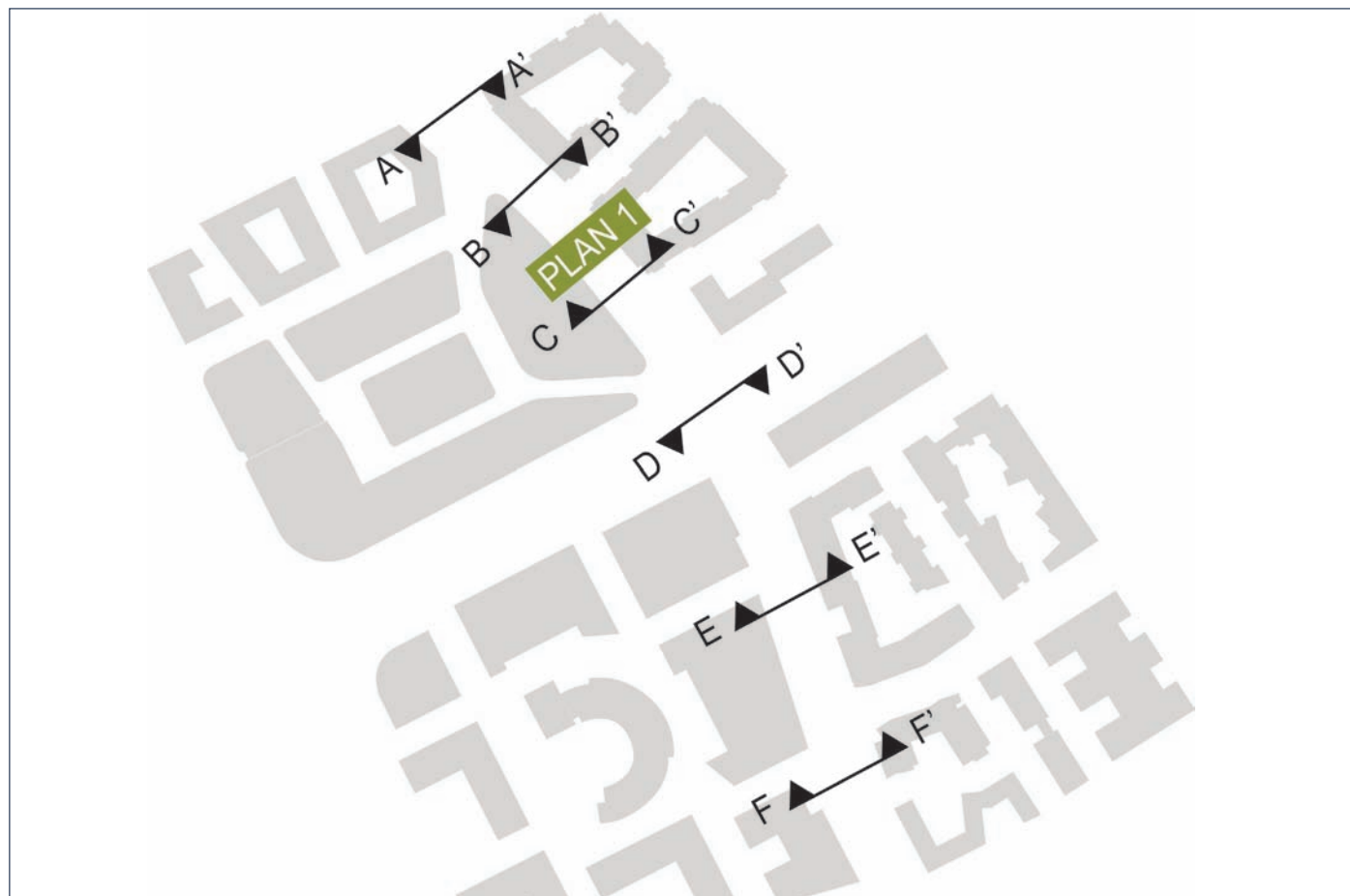


Figure 16: Key

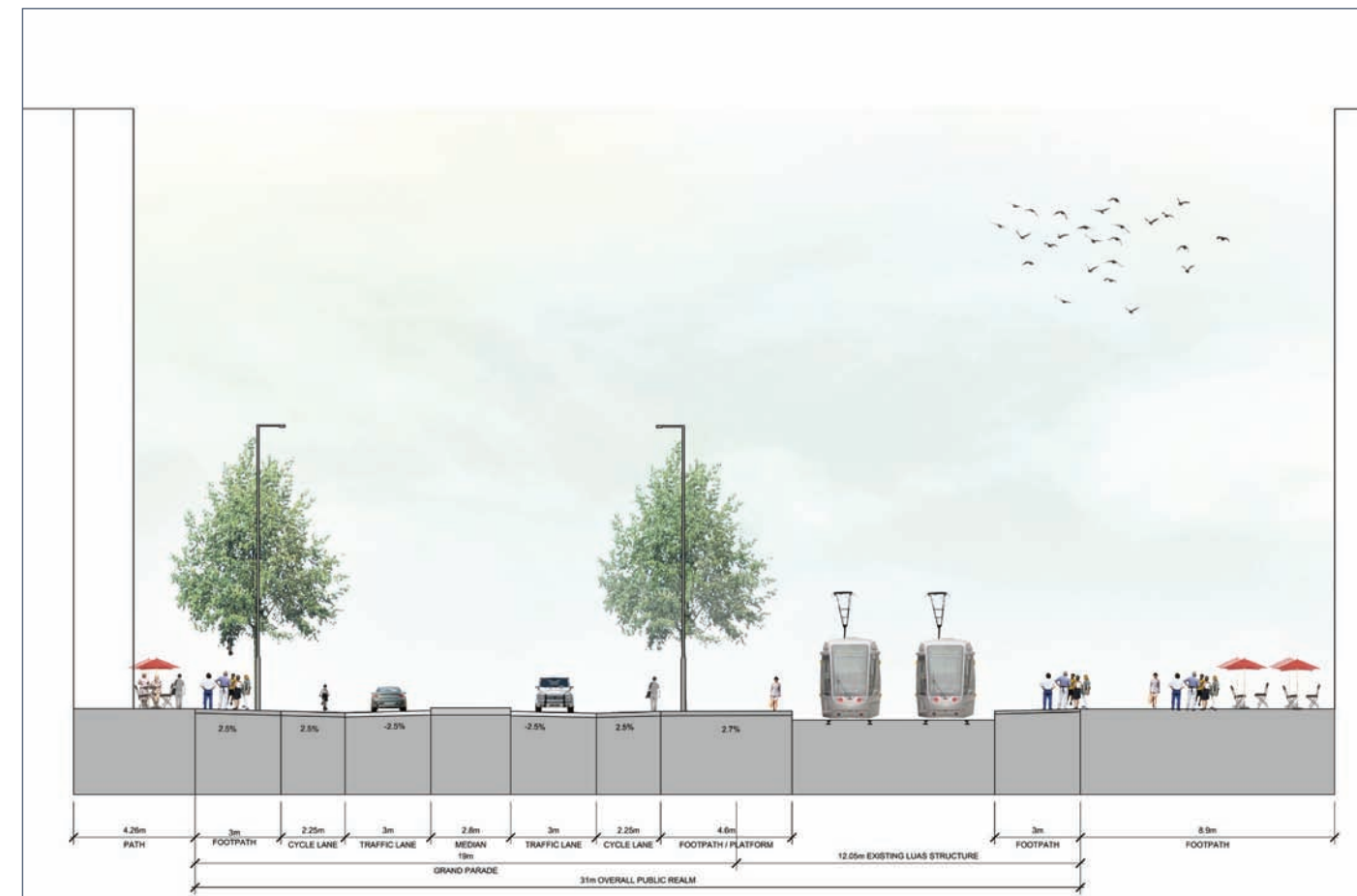


Figure 16a: Grand Parade: Section A-A TC1/TC2.

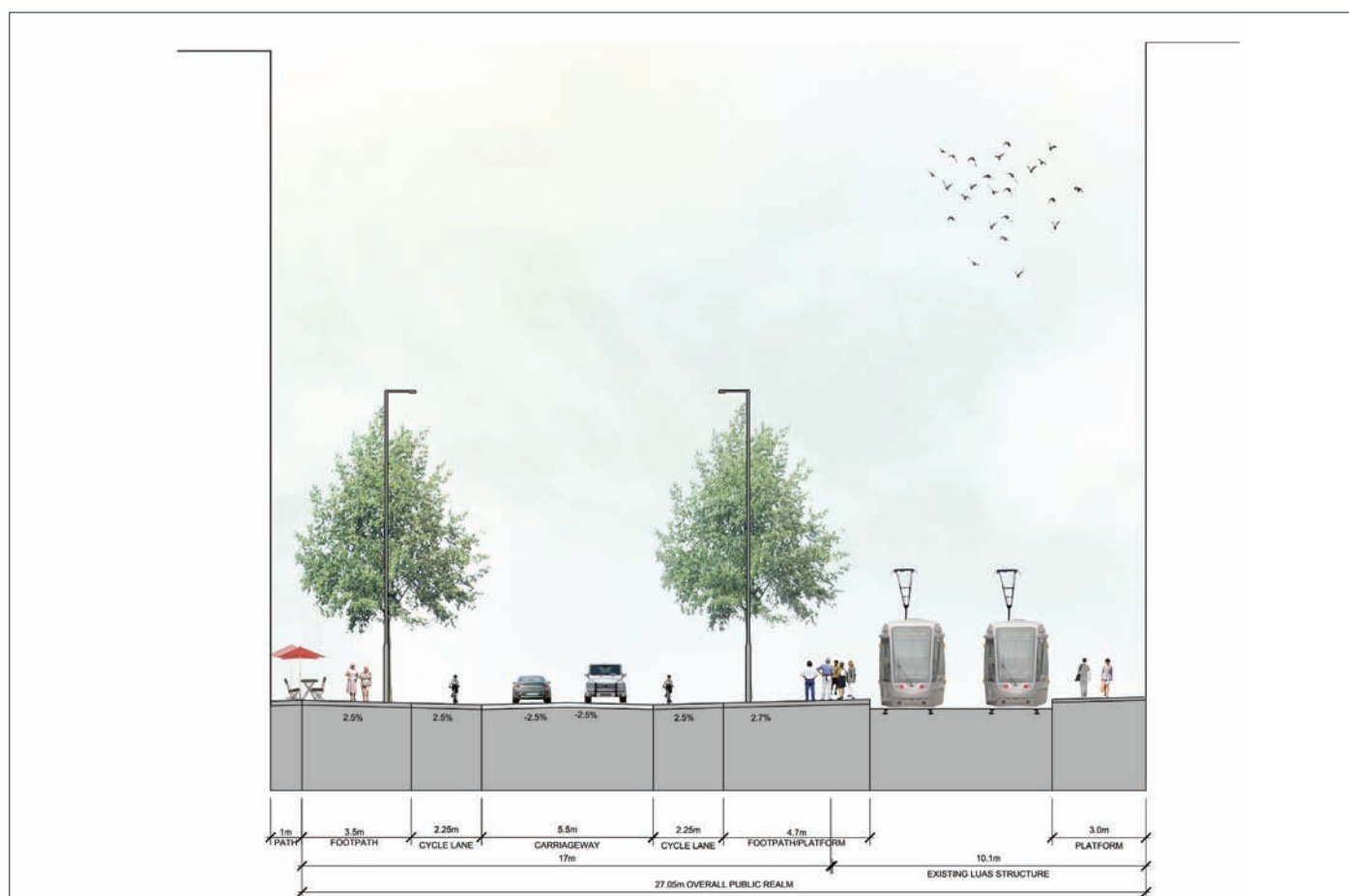


Figure 16b: Grand Parade: Section B-B TC1/TC2

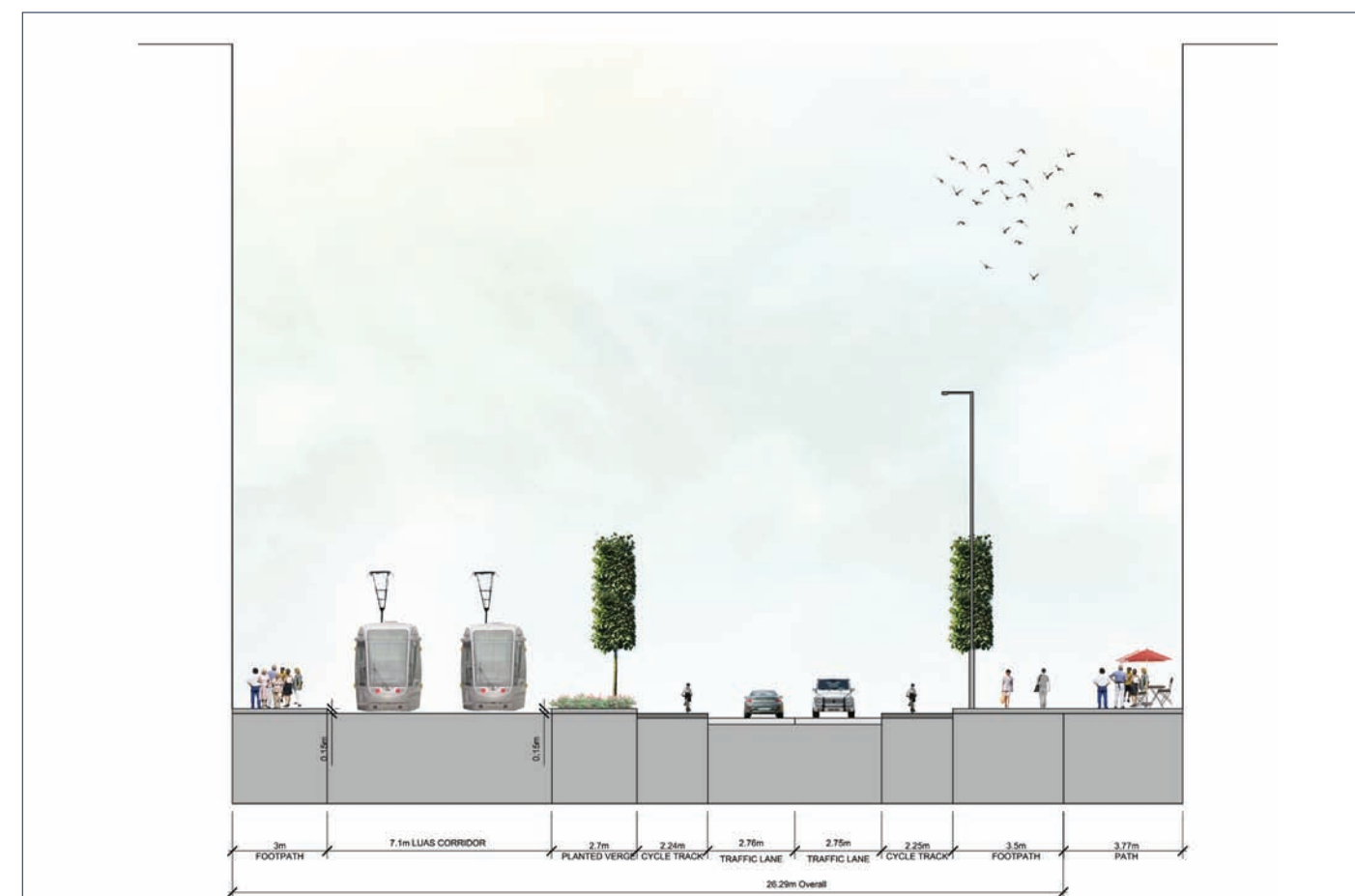


Figure 16c: Grand Parade: Section C-C TC3/TC4.

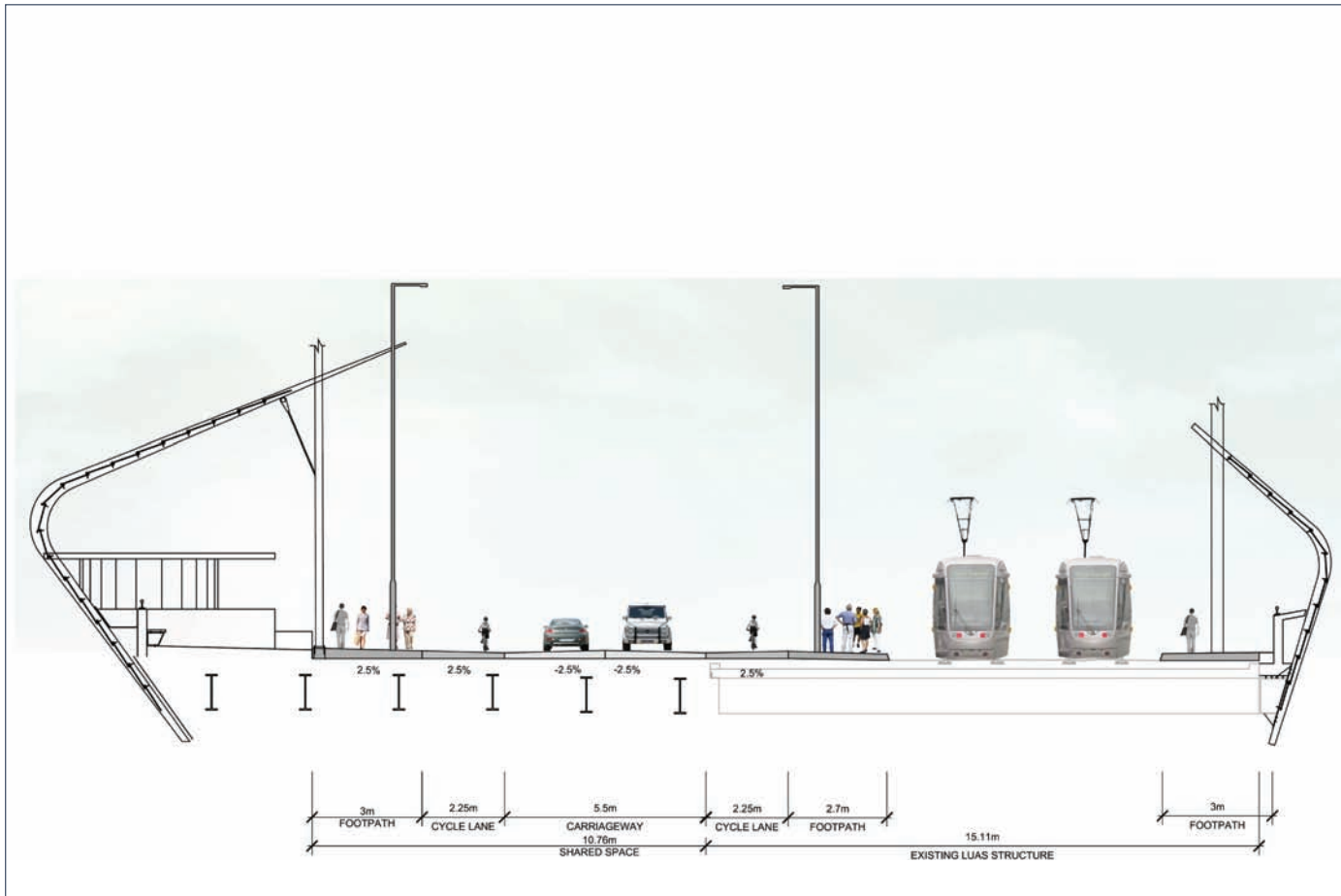


Figure 16d: Grand Parade: Sections D-D TC3/TC4.

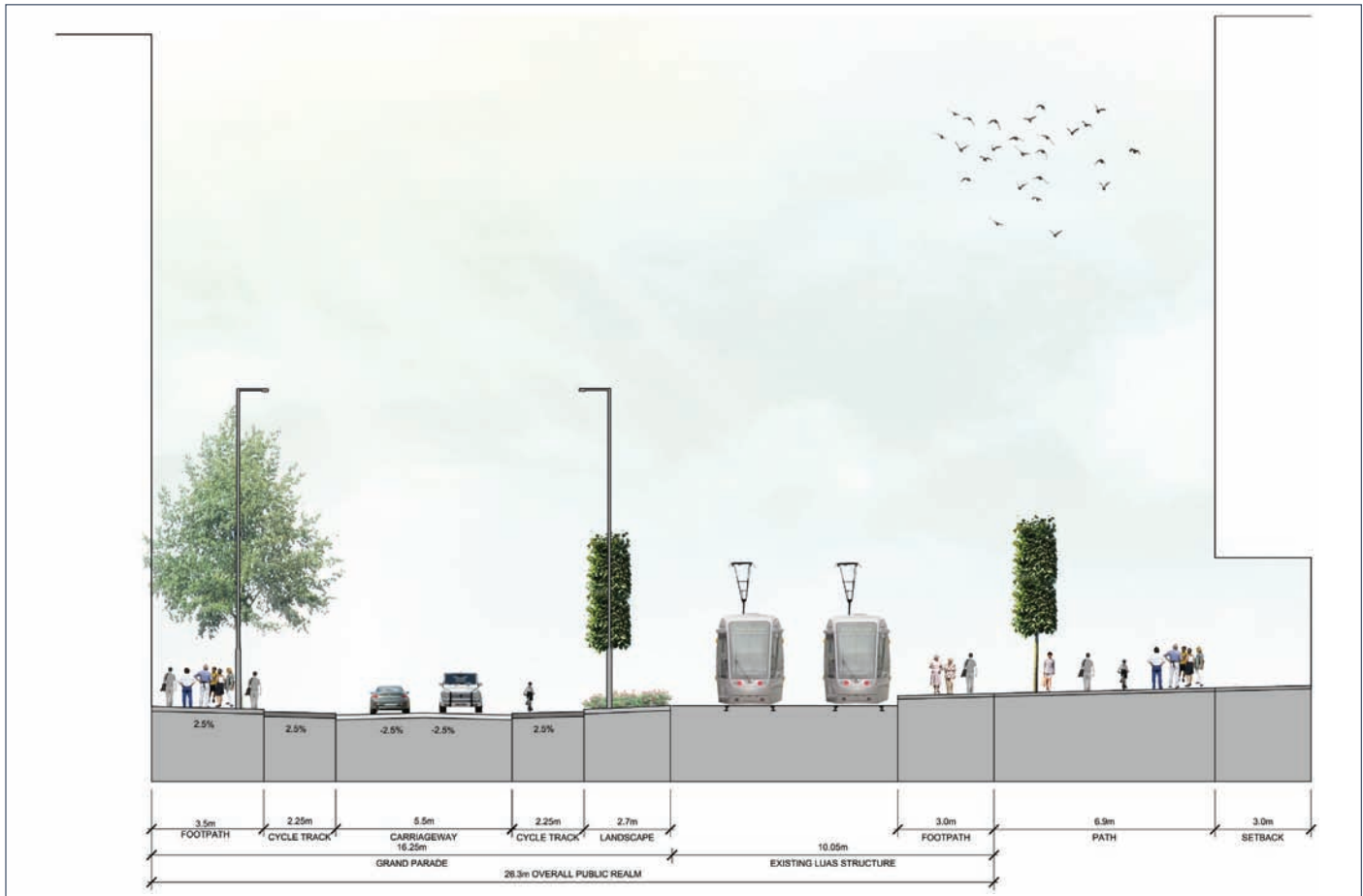


Figure 16e: Grand Parade: Section E-E TC3/TC4

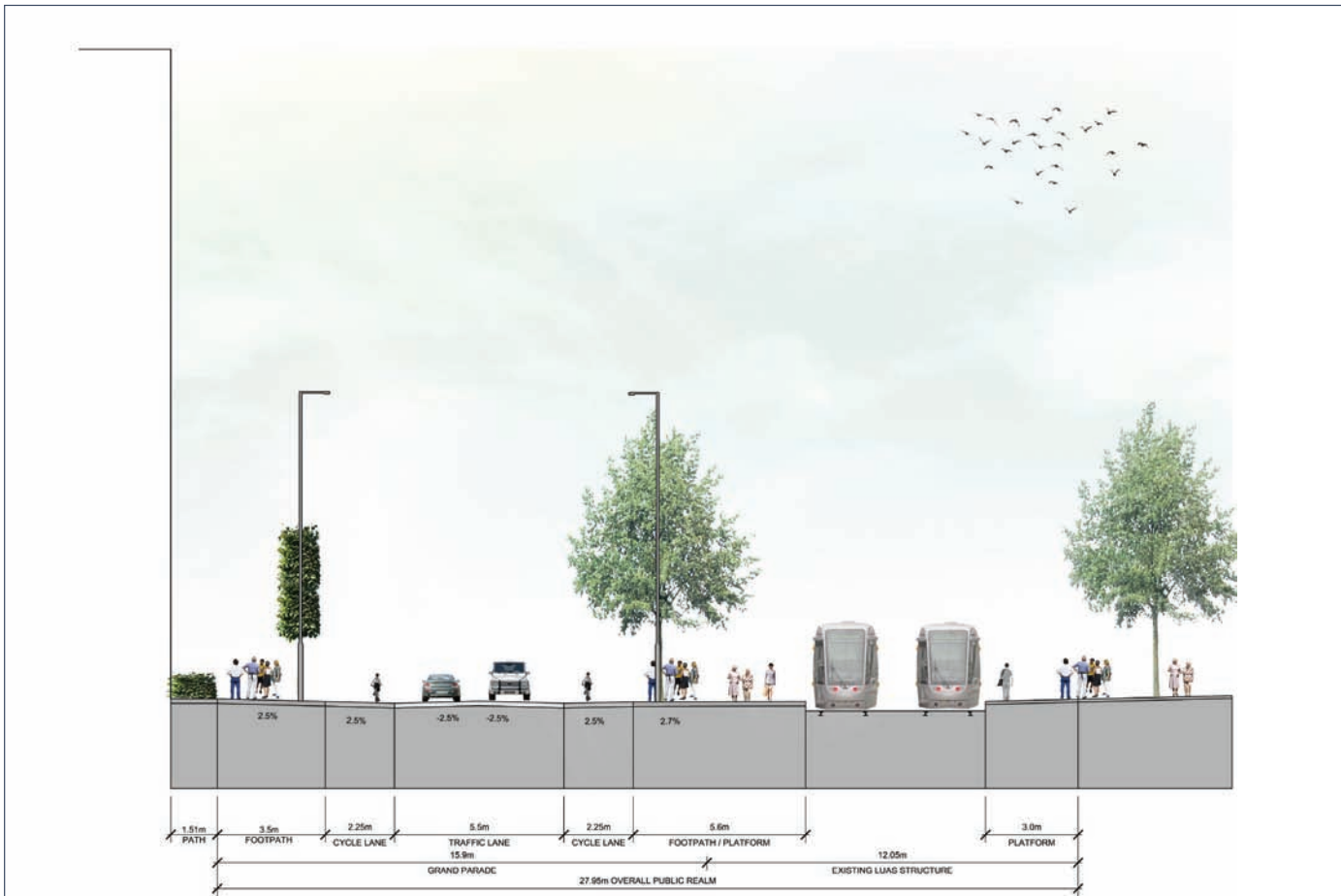


Figure 16f: Grand Parade: Section F-F TC3/TC4.

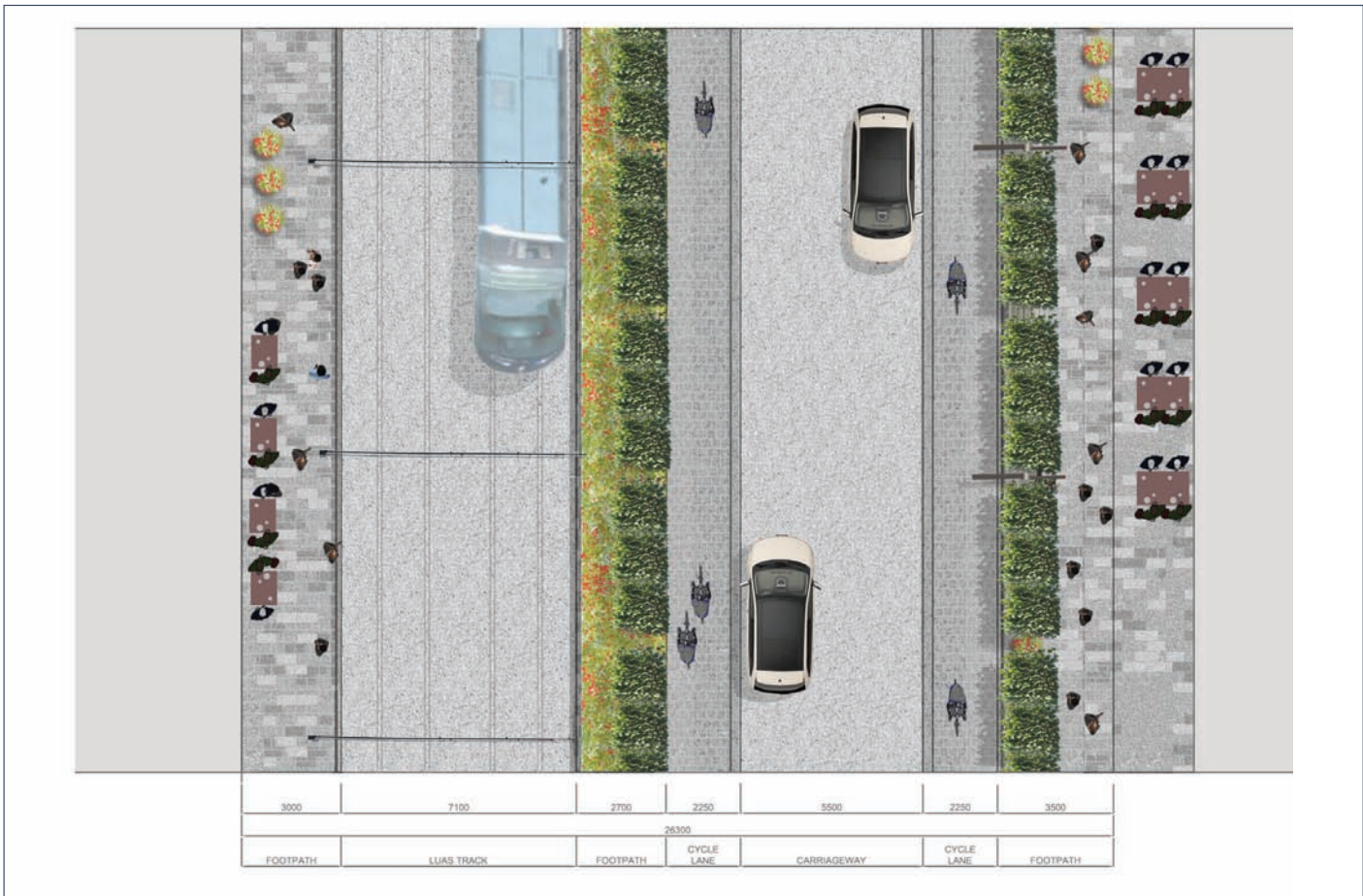


Figure 16g: Plan 1: Landscape Typical Detail (Section Grande Parade TC1/TC2).

4.2 Primary Civic Spaces

4.2.1 Cherrywood Square, Tully Park Link and Transport Interchange (No. 2 Map 6)



Figure 17: Cherrywood Square, Bishop Street Junction, Tully Park Entrance and Transport Interchange.

Cherrywood Square shall be designed as a spatially coherent, legible and highly accessible civic space, anchored by its location at the Cherrywood Luas stop, this space is the key arrival point to the Town Centre from the North. The Square will provide a visual and physical connection to both Tully Park and the Transport Interchange, both located to the north of Bishop Street Junction. This major civic space shall mark the presence of, and form the gateway to, Cherrywood Town Centre enabling direct and easy access to the principal focus in TC1, the civic and residential areas within TC1, Grand Parade to the south, and to Tully Park and the Transport Interchange to the north via three at-grade pedestrian crossings. In order to achieve a space, same design principles, treatment and quality shall apply to Tully Park entrance as per the plaza space at Cherrywood Luas stop. Complementary landmark sculptural or focal elements shall be located within each space to provide animation and visual interest, but also to support a strong visual connection and aid in mitigating the severance created by Bishop Street Junction. The Luas stop plaza area, adjacent to the Luas platform, focuses pedestrian and cycle access towards the Town Centre and to-and-from the

main access to Tully Park. In addition to the proposed Transport Interchange located to the north of the Town Centre, elements of Transport Interchange functions for taxis and soft modes may also be provided within Cherrywood Square. The Transport Interchange shall adopt the same paving strategy as Tully Park entrance and the plaza area (See Figures 17, 18a & b).

Cherrywood Square shall provide for a vibrant mix of uses with active frontages such as cafés and restaurants along its perimeter, with external seating at street and terrace level, both temporary, associated with the active uses and also permanent, as an integral element of the street space. The Cherrywood Luas platform shall be seamlessly integrated and provide for future extension of Luas platforms to 90m (if required). The Luas platforms will be integrated with the surface materials of the Square and remain at-grade with its finished levels, with the north end of the platforms tapering down gently to meet Bishop Street level and the southern end of the platforms rising to the level of Grand Parade.



Figure 18a: Cherrywood Square (Illustration)



Figure 18b: Cherrywood Square (Illustration)

The surface material on the Square shall extend across Bishop Street towards Tully Park and the Transport Interchange, in order to reinforce the strategic link between the those two attractors, the Town Centre and Cherrywood Luas stop.

4.2.2 Wyattville Link Plaza & Bridge (No. 3 Map 6)

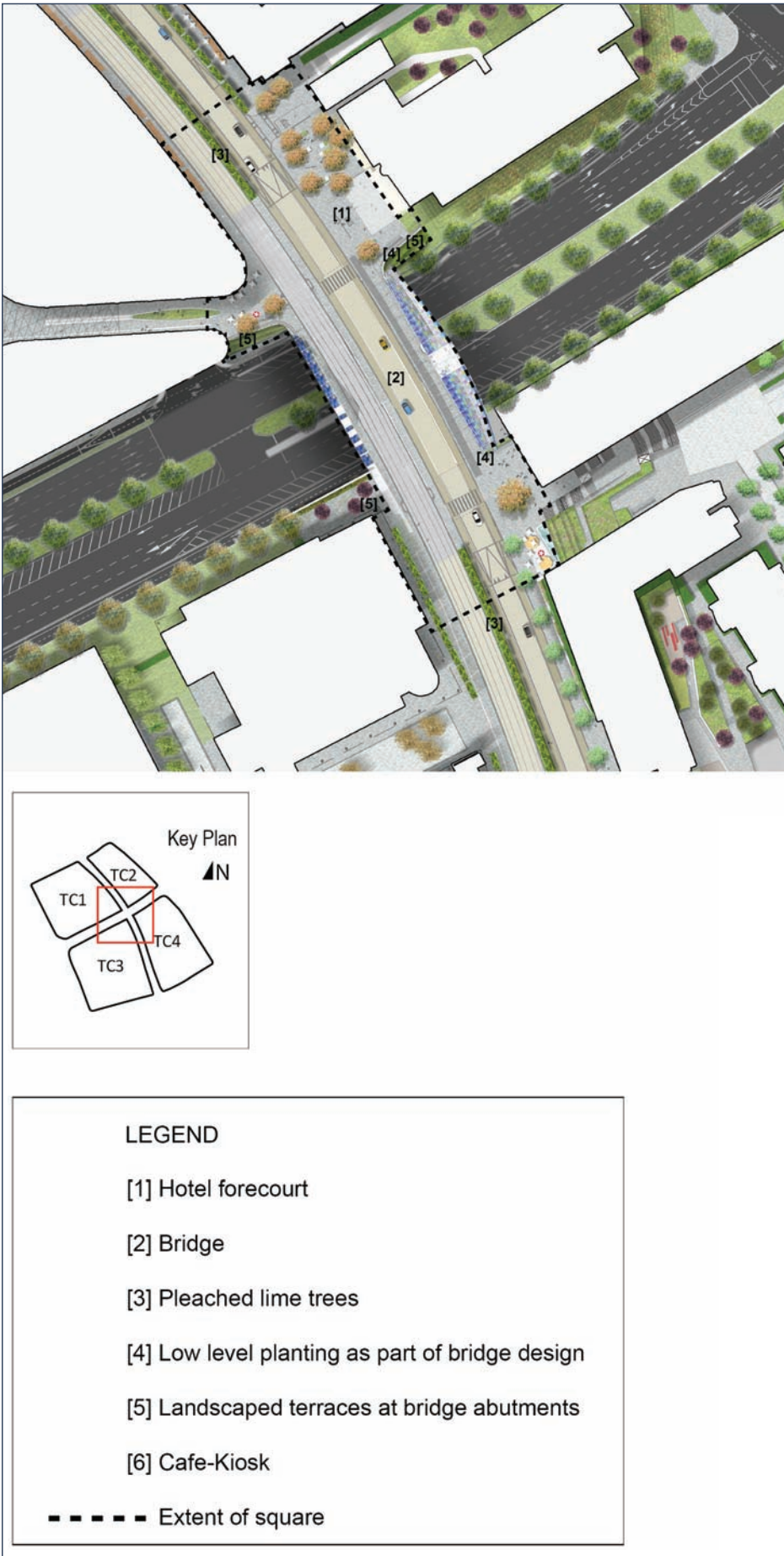


Figure 19: Wyattville Link Plaza and Bridge

Section 6.2 of the Planning Scheme, Objective DA 11: The Town Centre link should form an important public space of a civic character, with trees and soft landscaping elements for shelter and visual amenity. The design of the Town Centre Link shall incorporate features to protect pedestrian and cycle movement. These features will provide a safe environment for all users of the Town Centre Link and the Wyattville Link Road.

The Town Centre Link, namely the Wyattville Link Plaza and Bridge element of the Grand Parade is critical to enhance connectivity between the four quadrants of the Town Centre.

The design and architectural expression of the Wyattville Link Plaza and Bridge shall focus on four main functions as detailed below and illustrated in Figure 20a:

- **Crossing:** Safe and seamless crossing of the Wyattville Link Road for pedestrians, cyclists, vehicles and the Luas;
- **Protection:** Shelter from the wind and rain;
- **Place:** An attractive civic space for all users, as a destination space and also as an important orientation point;
- **Continuity:** Streetscape continuity along Grand Parade by (i) a structure of significance reflecting the scale and height of adjoining buildings and (ii) connectivity in terms of treatment of the landscaping, paving materials, lighting elements and street furniture.

The conceptual images of the plaza and bridge are for illustrative purposes only, any proposals shall incorporate the principles as shown in Figures 20b-d and 21, and shall demonstrate how the functional requirements shall be addressed, including the continuity of surface materials and the provision of enclosure across an otherwise environmentally challenging space created by the existing Wyattville Link Road. The plaza and bridge shall provide for a central café/kiosk and landscaped seating area; with secondary spaces to each corner of the bridge structure. The design shall also ensure that the corner elements of the bridge structure successfully integrate with the surrounding built environment and the bridge abutments shall be in the form of landscaped terraces of vertical green wall structures, or as otherwise agreed by the Planning Authority. In essence, the Wyattville Link Plaza and Bridge shall establish strong connections between TC1, TC2, TC3 and TC4.

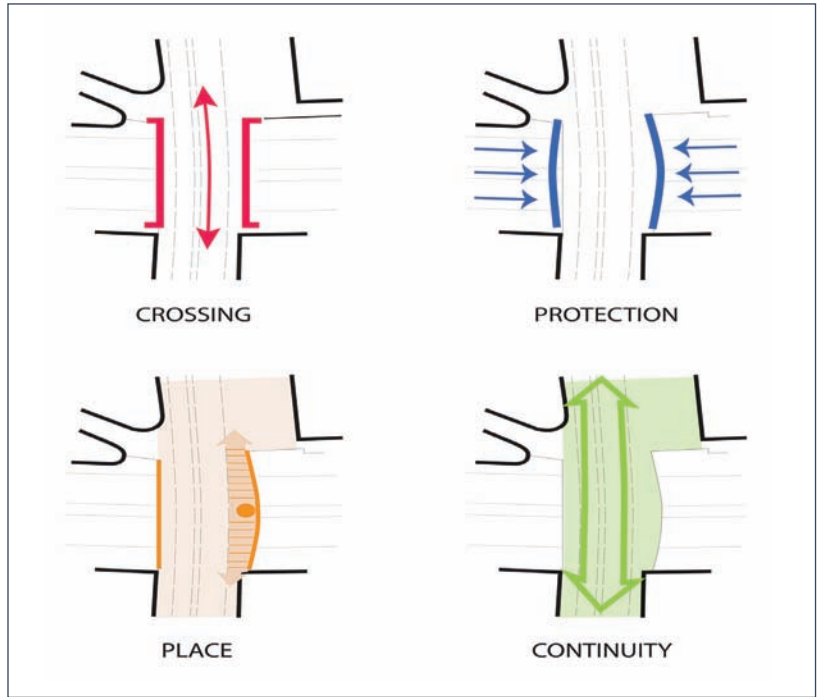


Figure 20a: Wyattville Link Plaza Bridge Function

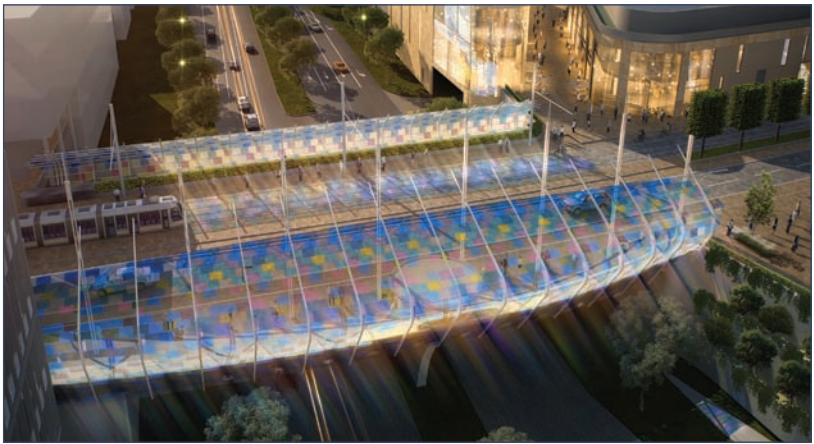


Figure 20b: Wyattville Link Plaza and bridge abutments (Illustration)



Figure 20c: Wyattville Link Plaza and Bridge (NW) (Illustration)



Figure 20d: Wyattville Link Plaza and Bridge (SW) (Illustration)



Figure 21: Wyattville Link Plaza and Bridge along Grand Parade (Illustration)

4.2.3 Brides Glen Square (No. 4 Map 6)

Brides Glen Square is a Primary civic space centred around Bride's Glen Luas stop (Figure 22). The Square is located at the intersection of Grand Parade and two pedestrian streets, connecting in an easterly direction to TC4 and the existing Business Park and in a westerly direction to TC3 and Macnebury (Development Area 7 of the Planning Scheme). The Square is framed on its north-eastern and south-western flanks by taller buildings utilising upward modifiers (See Map 5, Section 3.3.2). The purpose of these upward modifiers is to signal the presence of and provide a greater degree of enclosure and definition to the civic space which stretches across the Grand Parade and the Luas stop.

A mix of uses to provide active frontage shall be provided around the Square, such as cafés and restaurants with external seating, retail units, and office / building entrance lobbies.

As part of Grand Parade, Brides Glen Square will continue the 'urban' character in terms of quality materials, finishes, landscape and street furniture. This space shall provide for a focal point in the form of appropriate sculpture, public art or similar to mark the gateway to Cherrywood from the south.

The Bride's Glen Luas stop platforms will be integrated with the surface materials of the Square and remain at-grade with its finished levels, with the platforms tapering gently to meet the levels along Grand Parade and at each end. Future extension of Luas platforms to 90m (if required) shall be provided if necessary.



Figure 22: Brides Glen Square

4.3 Secondary Civic Spaces

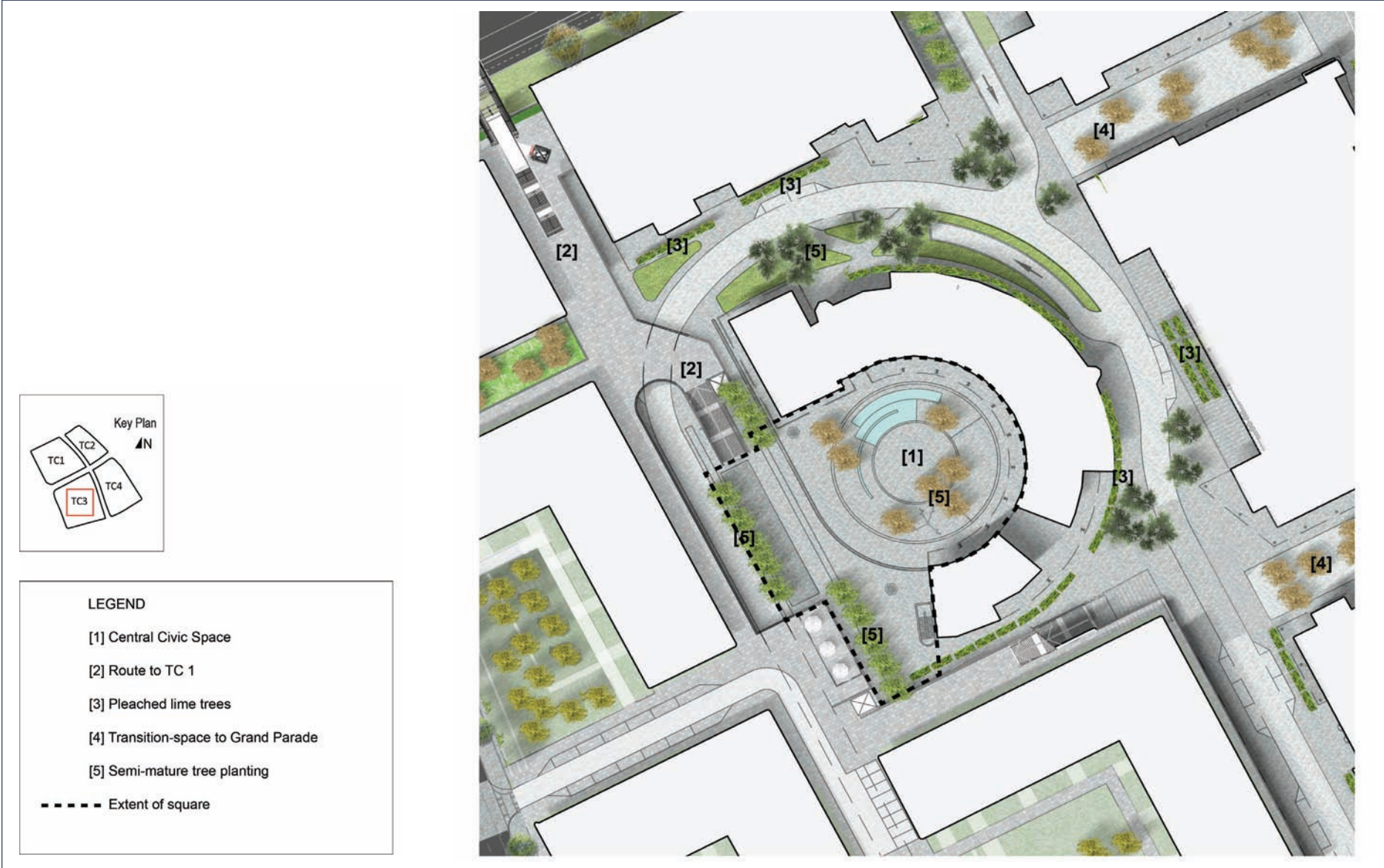


Figure 23: TC3 Central Civic Square

4.3.1 TC3 Civic Square (No. 5 Map 6)

TC3 shall provide a central civic space as a focal point for the broad mix of activities and uses which will animate this part of the Town Centre throughout the day and evening (Figure 23). The community use quantum designated in the Planning Scheme for TC3 shall relate to this space, as well as a variety of retail and non-retail uses. It is envisaged that it will be a flexible space with people relaxing or working at cafés and restaurants, at other times vibrant with activity such as performances, markets and attractions. This shall be reflected in the typology of buildings where design diversity shall reflect the range of functional uses, whilst also achieving an overall coherence through the use of materials, finishes, street furniture and landscape design. The location is highly connected to TC1, TC2, TC4, Macnebury and the wider area (Figure 23a).



Figure 23a: View of TC3 Central Civic Space (Illustration)

Planting in the form of mature evergreen trees will define the transition spaces between Grand Parade and central civic core, this stand of mature native trees will provide opportunities for privacy, a space to sit and relax in a calming environment while contributing to a requirement of balance and human scale within this quadrant. The transition spaces in TC3 between Grand Parade and the central civic space shall be planted with areas of soft landscaping that counterbalance the hard built environment to provide an attractive, pleasant place for people to sit and relax, that varies in nature with the seasons.

The UFDF provides for the pedestrian and cycle link across Wyattville Link Road connecting TC3 and TC1 as required by Section 6.2 (h) and in accordance with Map 2.4 of the Cherrywood Planning Scheme. This pedestrian and cycle link shall, in its design and treatment, provide legibility and passive surveillance, with the use of transparent materials, quality lighting, be generous in size with a wide stepped access, large landing areas and oversized lifts to accommodate multiple bicycles at both TC3 and TC1 entrance and exit points. The link is of superior design in both form and function with a consistent architectural identity throughout that is similar to the Grand Parade Bridge across the Wyattville Link Road (Figure 24). This bridge provides an important pedestrian/cycle link between TC1 and TC3 and as such the sequencing of its delivery is equally important. The landing points for the bridge shall be included in the relevant landowner’s planning application for the development immediately adjacent/abutting the bridge. The central median support, to be located on the Wyattville Link Road, shall be included in the first of the development proposals adjoining the bridge structure. The remainder of the bridge structure shall be delivered by the first phase of adjoining development in the opposing quadrant. As such these elements shall be included in the relevant planning applications.

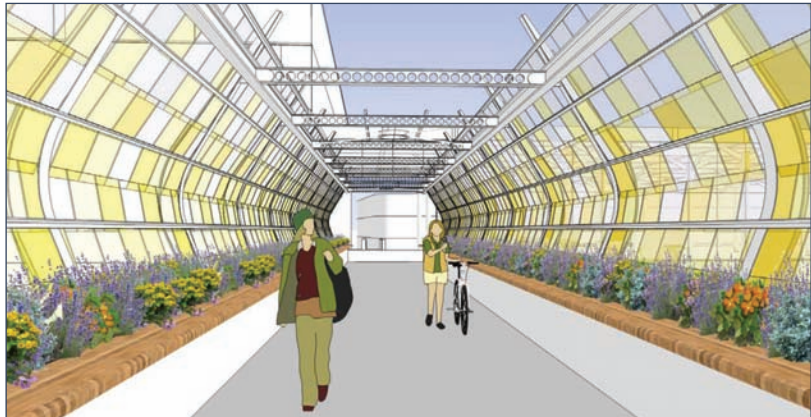


Figure 24: TC1 / TC3 Pedestrian/cycle link (Illustration)

4.3.2 Town Square East and West (No. 6 & 7 Map 6)

The Town Square is located in TC1 and essentially consists of 2 no. civic spaces, Town Square East and Town Square West, located at the intersection of primary pedestrian and cycle streets traversing the Town Centre; notably from the Cherrywood Luas stop and Grand Parade (east); TC3 (south); Tully Park (north) and Macnebury (east). The Town Square shall function as a key civic space centred around the focal point of retail, retail services and leisure activities (Figure25).

Town Square West shall be open at its four corners allowing multiple pedestrian and cyclist routes to converge towards its centre, reminiscent of the traditional “market square” in European towns and villages. The space shall provide areas to sit and linger and shall also accommodate flexible uses such as temporary markets, performances and street festivals. It shall have an environment that feels external through the use of appropriate building, façade and shop front design, street furniture, and surface materials where

pedestrians and cyclists feel at ease to navigate through the space. The Square shall be oriented to optimise sunlight penetration so as to create attractive sunny spill out spaces for cafés, restaurants and outdoor entertainment. Floor openings and modes to descend to lower levels shall be sensitively sized and located so as not to dominate the space and allow for pedestrian and cyclist permeability.



Figure 25: Town Square West TC1 (Illustration)

4.3.3 Bishop Street Square & Tully Park Link (No. 8 & 9 Map 6)

Bishop Street Square is a Secondary civic space which forms a secondary link between the Town Centre and Tully Park, located at the junction with Bishop Street (Figure 26). The Square, surrounded by residential buildings, shall essentially comprise of a tree-planted pocket park or square and as such shall create a community space to sit and relax. This space shall also provide a visual link which gently orientates pedestrians towards the pedestrian crossing on Bishop Street. The surface material of the square will extend across Bishop Street towards the Tully Park Link in order to reinforce the visual and physical link between the Town Centre and Tully Park.

The Tully Park Link allows for easy and safe pedestrian movement from Tully Park and the residential quarters to the north of the Town Centre to the principal focus and civic core and retail focus in TC1. Tully Park Link shall be universally accessible for pedestrians and cyclist and shall provide direct and safe access from Tully Park to Bishops Street Square. The route alignment and detailed design of this link shall be devised in conjunction with the adjacent plots.



Figure 26: Bishops Street Square

4.4 Streets - Pedestrian & Cycle

4.4.1 TC1 Pedestrian & Cycle Covered Streets

The opportunity to protect the pedestrian and cycle retail dominated streets of the Town Centre (TC1 Blocks B1-B6) from the elements, in particular wind tunnelling, shall be considered in its design. The design of any covering shall be in the form of a glazed canopy-type structure fixed to the parapet of the buildings to appear as a 'floating' structure (Figure 27). This structure shall provide partial covering (see Map 6 Public Realm Strategy for extent of covering) and shall protect the principal shopping streets from the excesses

of the climate while retaining their essentially external and open nature. All building finishes, paving materials, planting specifications, street furniture and lighting elements, shall be external in nature and provision shall be made for ingress of surface water through the installation of drainage. SuDs features shall be utilised throughout and appropriate irrigation systems installed in areas containing soft landscaping.

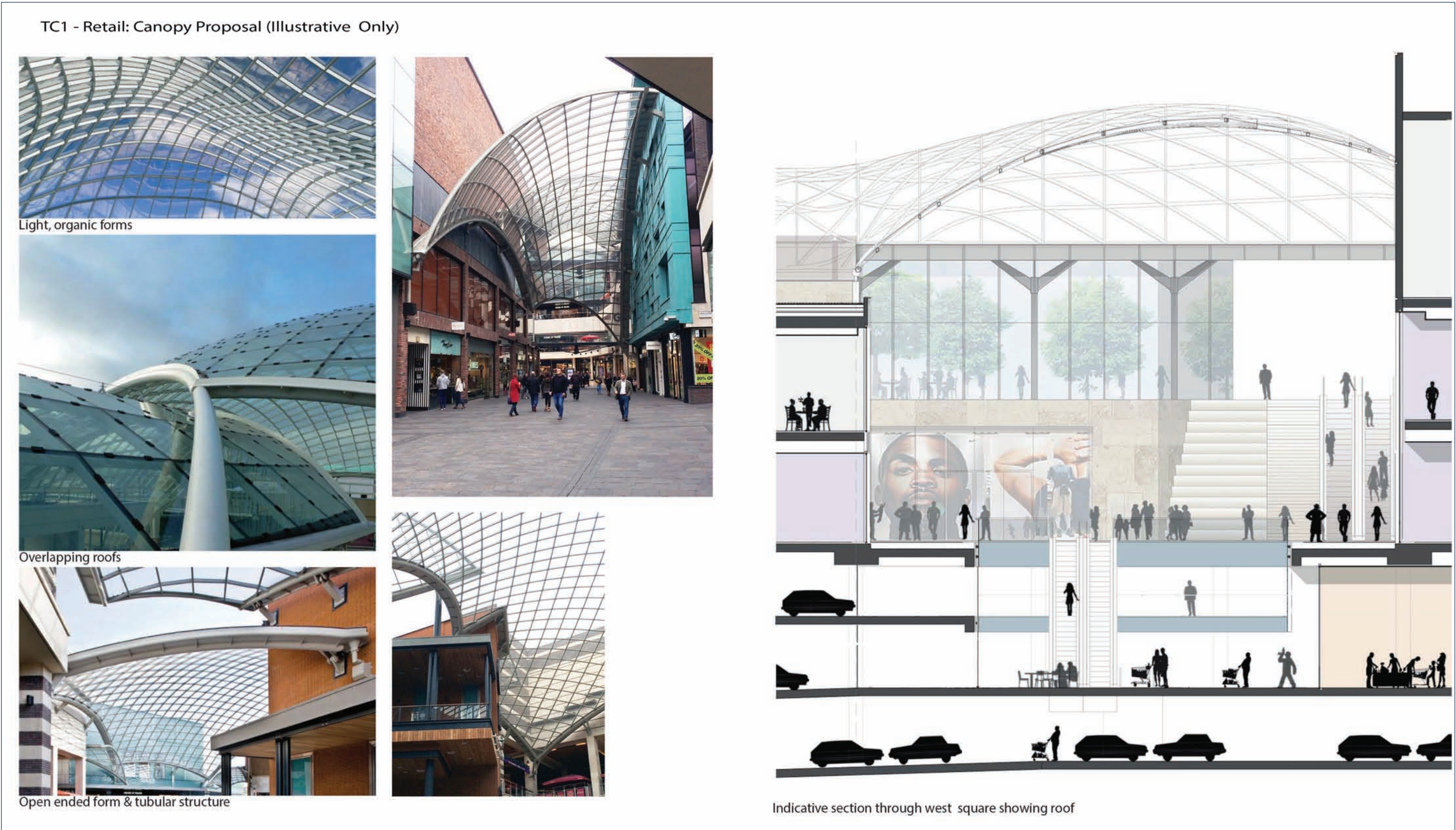


Figure 27: TC1 Retail Covered Streets (Illustration)

4.4.2 Internal Pedestrian and Cycle Street Network (Map 6)

Objective GI 34 of the Planning Scheme: 'That a network of permeable pedestrian routes and greenways shall link all areas with the Planning Scheme including Luas stops, bus stops, employment areas, schools, village centres, Town Centre, open spaces and green infrastructure'.

Objective DA 10 of the Planning Scheme states with regard to the design and layout of the Town Centre that: 'In relation to ground levels, the change in the levels between the surrounding road network and the Town Centre Link shall be designed in a manner that is attractive to the pedestrian'.

In the Town Centre, the pedestrian and cycle street network provides connections between residential areas and the retail, leisure and commercial core, as well as the supporting community facilities and parks, creating a sustainable compact neighbourhood. The street network shall be universally accessible in design and provide for layers of usage by incorporating focal elements and destinations, such as creative streetscape designs, safe environments with opportunities to transition, sit and play. The connections shall create a pleasant and convenient environment for pedestrians and cyclists.

The design challenge for the Town Centre is to reconcile the existing and new ground levels (See Section 3.2 and 3.4. of the UFDF) and this design challenge must be overcome to create permeable routes and connections throughout the Town Centre. The pedestrian and cycle streets connecting Tully Vale to Grand Parade through TC2 and those connecting Cherrywood Business Park and Grand Parade through TC4 are particularly challenging in terms of level changes. However, it is essential that these links are legible, attractive and with an appropriate degree of passive supervision. Building orientation, access, surface materials and planting will be important factors in creating attractive streets as part of the public realm network (Figure 28a & b).



Figure 28a: Civic Street TC1 (Illustration)



Figure 28b: Intersection at Grand Parade with internal street TC4 (Illustration)



4.5 Materials, Finishes & Street Furniture in the Public Realm

In accordance with Objective PD 14 `To ensure that distinctiveness of materials is used at various scales, allowing for a coherent and high quality built environment with an individual palette to identify each neighbourhood, high quality finishes are to be used in the public realm`.

The materials and finishes included as part of any hard and soft landscaping of the Town Centre streets, civic spaces and courtyards shall be of high quality and include a combination of natural stone and reconstituted artificial stone with colours ranging from warm beige to light and darker greys (See Figure 29a-c Palette of Materials). The resultant palette of materials shall support the requirement for a legible public realm with a hierarchy of spaces through the use of different stone types, sizes, colours and finishes.

Materials for road finishes along Grand Parade’s primary civic spaces and the links across Bishop Street to Tully Park, shall be of superior quality and shall define Grand Parade as a pedestrian and cycle friendly space, whilst still ensuring that these areas are suitable for Taking in Charge (TIC) by the Local Authority if required, and meet the requirements of Transport Infrastructure Ireland (TII). Along this route a uniform and for the most part, a flush surface shall ensure universal accessibility using materials, street furniture and planting to define spaces.

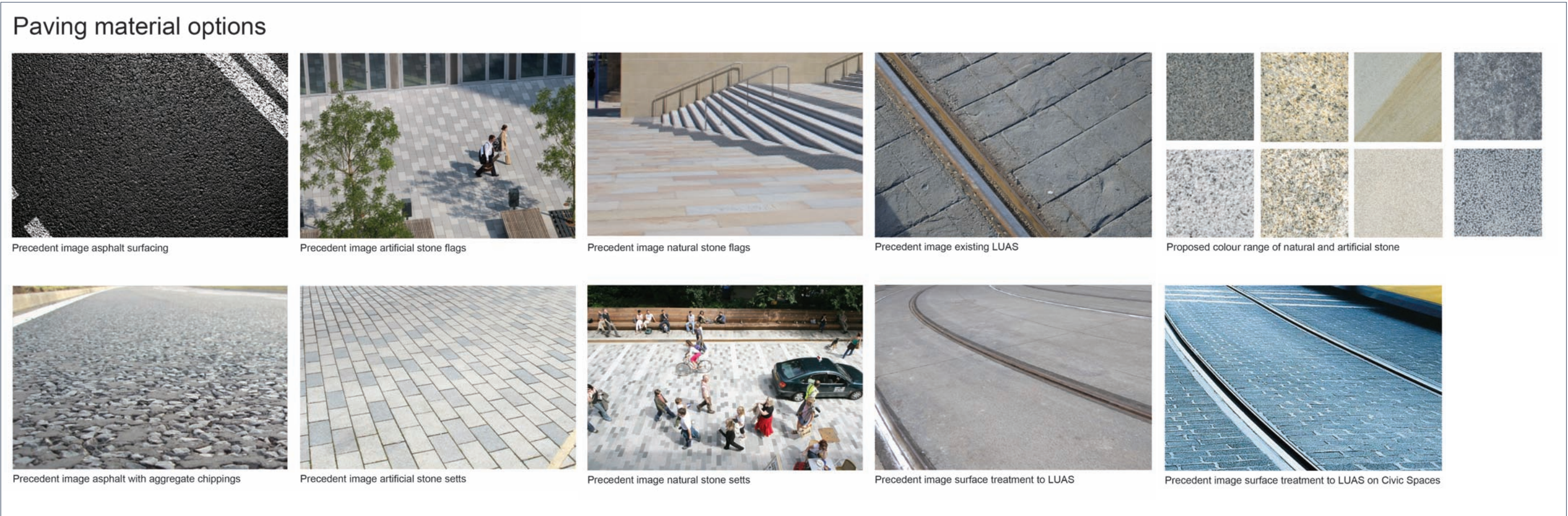


Figure 29a: Palette of materials for primary & secondary routes and squares (Illustration)

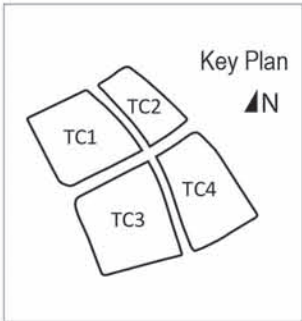
Paving material options

Surface Area

Grand Parade Carriageway:
 Grand Parade Cycle Paths:
 Bishop Street Junction:
 Pedestrian Crossing Point at Tully Park Entrance:
 Civic Spaces:
 Footpaths:
 Luas trackbed:
 Luas trackbed on Civic Spaces:

Material Options

Asphalt, Asphalt with aggregate, Artificial stone pavers (setts), Natural stone pavers (setts)
 Asphalt, Asphalt with aggregate, Artificial stone pavers (setts), Natural stone pavers (setts)
 Asphalt, Asphalt with aggregate
 Asphalt, Asphalt with aggregate, Artificial stone pavers (setts), Natural stone pavers (setts)
 Artificial stone pavers (flags & setts), Natural stone pavers (flags & setts)
 Artificial stone pavers (flags & setts), Natural stone pavers (flags & setts)
 Retain existing surface, Surface Treatment, Artificial stone pavers (Flags & Setts), Natural stone pavers (Flags & Setts)
 Retain existing surface, Surface Treatment, Artificial stone pavers (Flags & Setts), Natural stone pavers (Flags & Setts)



LEGEND

Footpaths, Civic Spaces, Paved Crossing Points

Grand Parade Carriageway

Grand Parade Cyclepaths

Luas - Surface Treatment on Civic Spaces

Luas - Surface Treatment

Figure 29b: Palette of materials for primary & secondary routes and squares

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Paving material options

Surface Area

Grand Parade Carriageway:
 Grand Parade Cycle Paths:
 Bishop Street Junction:
 Pedestrian Crossing Point at Tully Park Entrance:
 Civic Spaces:
 Footpaths:
 Luas trackbed:
 Luas trackbed on Civic Spaces:

Material Options

Asphalt, Asphalt with aggregate, Artificial stone pavers (setts), Natural stone pavers (setts)
 Asphalt, Asphalt with aggregate, Artificial stone pavers (setts), Natural stone pavers (setts)
 Asphalt, Asphalt with aggregate
 Asphalt, Asphalt with aggregate, Artificial stone pavers (setts), Natural stone pavers (setts)
 Artificial stone pavers (flags & setts), Natural stone pavers (flags & setts)
 Artificial stone pavers (flags & setts), Natural stone pavers (flags & setts)
 Retain existing surface, Surface Treatment, Artificial stone pavers (Flags & Setts), Natural stone pavers (Flags & Setts)
 Retain existing surface, Surface Treatment, Artificial stone pavers (Flags & Setts), Natural stone pavers (Flags & Setts)

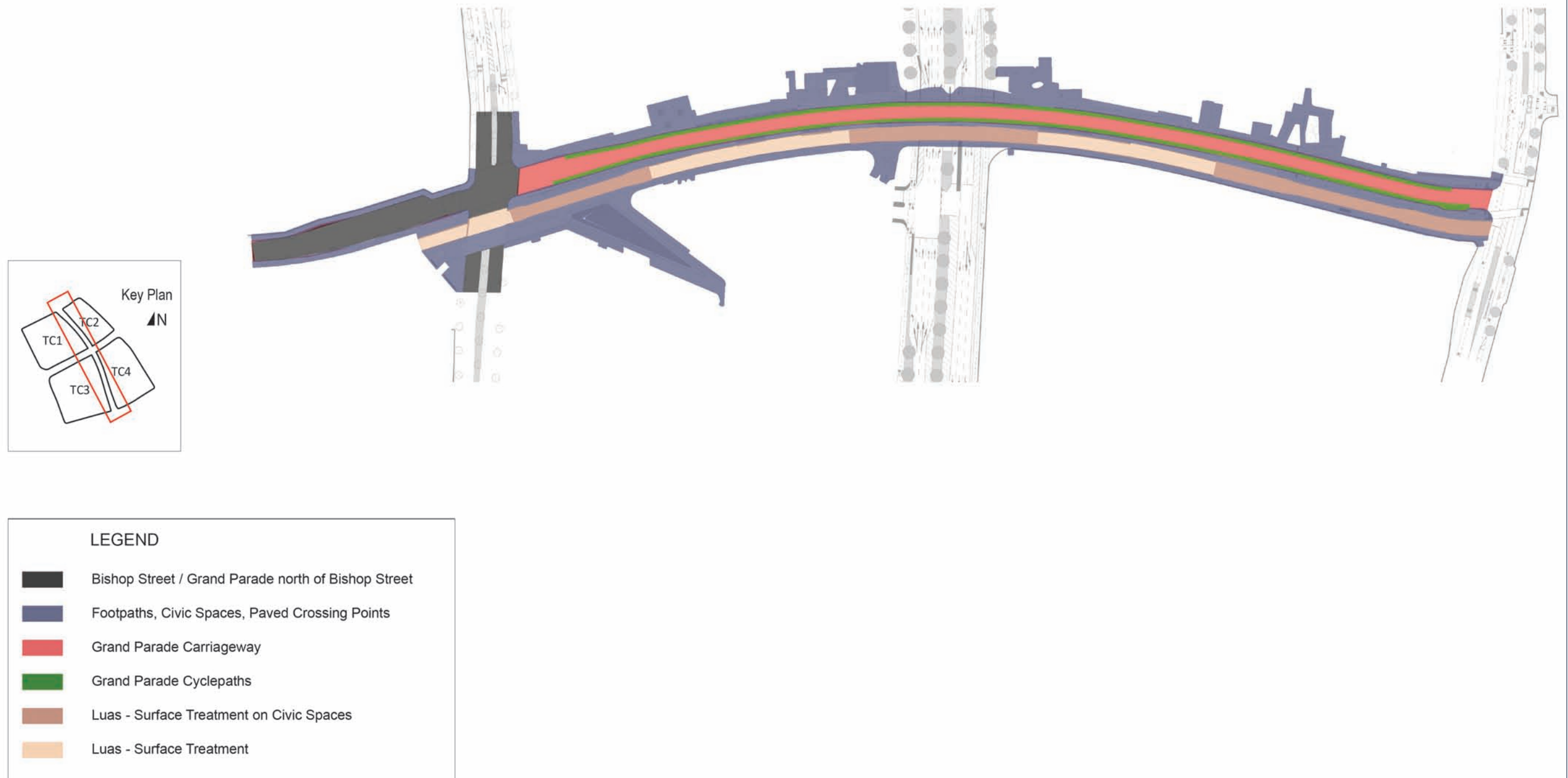


Figure 29c: Palette of materials for Grand Parade

A variety of seating opportunities shall be provided throughout the public realm of the Town Centre. These may be in the form of feature steps, planter edges or walls, whilst innovative designs shall reduce the scope for street furniture clutter. Café and restaurant seating shall be provided along building frontages at suitable locations, but shall not dominate civic spaces that are required for multiple uses. Public lighting shall be site specific, utilising decorative lighting standards in plazas and squares. All street furniture shall be cohesive, high quality and site specific. Cycle stands shall be appropriately located to ensure that it is easily accessible and yet does not detract from usable public spaces, and in this regard, innovative cycle parking solutions shall be required to provide for the required quantum whilst integrating seamlessly with the public realm. Meanwhile, focal point in the form of water

features, public art or otherwise shall be incorporated in the civic spaces appropriately and shall be unique to each space.

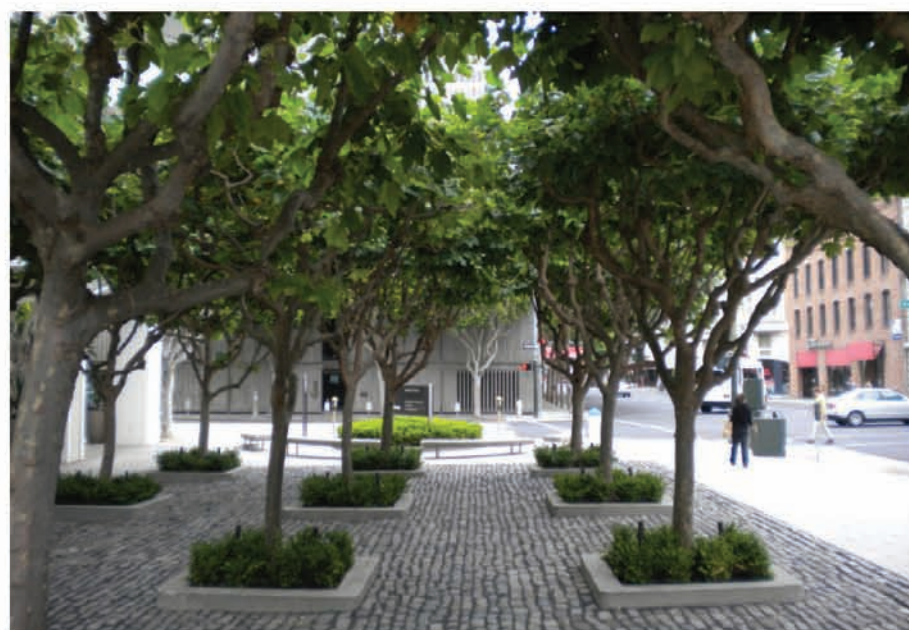
Town Centre streets and squares shall be designed to accommodate both low level planting and regular tree planting to define squares, routes and principal frontages. Block courtyards shall also be landscaped and accommodate low level planting and trees on a less formal layout. Along Grand Parade, trees shall create a formal curved avenue with punctuations in locations where the street opens up into squares and plazas.

Throughout the Town Centre, species selection shall be native in most part however plant selection shall be site specific to ensure appropriate species are planted in each space. Appropriate planting details shall be employed so

as to ensure suitable growing conditions and to protect adjacent street scapes / hardscape and any adjoining structures or services (Figure 30).



Pleached Lime



London Plane



Figure 30: Examples of feature street trees

4.6 Communal Amenity Space

All designs for communal amenity space, including open space, courtyards, roof terraces and communal residential pedestrian streets shall take cognisance of Section 5.4.4 of the Planning Scheme and in doing so comply with the 'Cherrywood Amenity Space Guidance Document 2017' and have regard to in particular Objective GI 11 as detailed below:

Objective GI 11 of the Planning Scheme states: 'To demonstrate that level changes necessary to accommodate development and associated infrastructure will not encroach on planned open space and will facilitate the protection of features for retention.'

4.6.1 Communal Residential Pedestrian Streets & Courtyards

The block layout for the Town Centre creates a hierarchy of streets with a series of communal residential pedestrianised streets and courtyards, accommodating pedestrians, cycle and emergency vehicles only (Figure 31). These shall be in the form of controlled access landscaped streets which provide additional communal amenity space for residents, access may be provided via controlled gates as well as from the adjacent apartment cores, and boundaries shall be open in nature and comprise of 'see-through' or 'open-fencing' with soft landscaping where necessary.

All spaces shall include a mix of high quality hard and soft landscaped areas and allow for multiple uses not dedicated to a single user group: these shall include seating areas and low-level planting with a variety to add visual interest and bio-diversity gains on a seasonal basis. The spaces shall also be designed to cater for informal activities, relaxation and play and in this regard, the design of courtyards should demonstrate their ability to be flexible spaces. In addition, level changes shall be utilised and sensitively incorporated to add interest, create play opportunities, contribute to the privacy of residents and support universal design accessible to all users. Integrated into landscape design at concept stage, level changes have the potential to create characterful spaces with dual functions of access and use.

Opportunity for play shall be provided in residential areas in accordance with the 'Design Standards for New Apartments' published by the DECLG, December 2015. Playful spaces shall be provided; reducing the reliance on play equipment by stimulating imaginative play, these will allow a flexibility of use therefore encouraging a range of people to use the amenity. Design solutions shall balance the needs of all residents by providing multifunctional uses within all amenity spaces.

4.6.2 Roof Gardens & Terraces

Roof gardens in the form of Intensive Green Roofs can provide another variety of amenity however should not be the sole amenity available, they shall complement ground level spaces and in some cases these can contribute to achieving higher standards and provide a variety of spaces. Intensive Green Roofs can contribute positively toward SuDs, whilst planting can mitigate wind, provide shelter, and privacy.

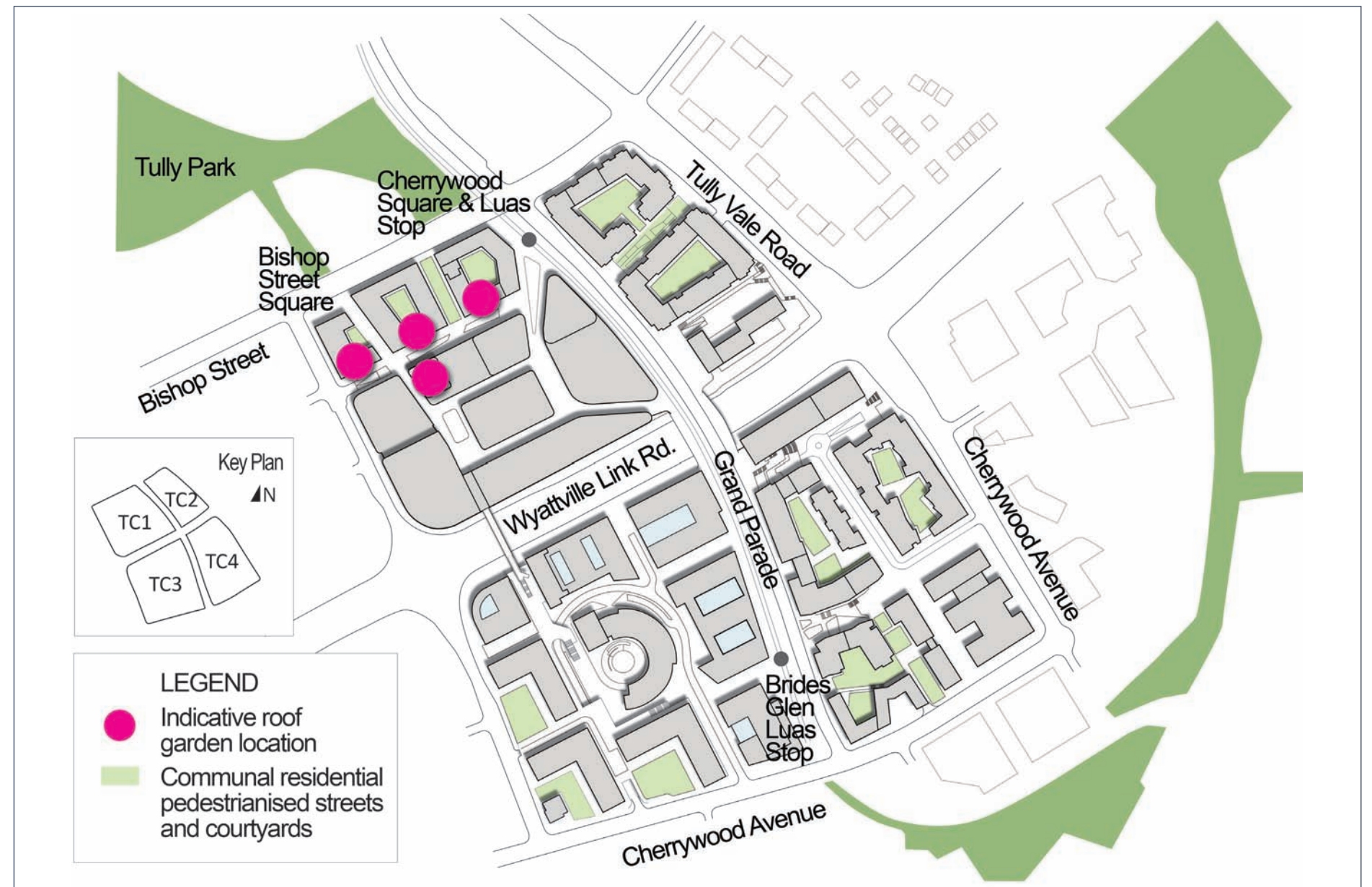


Figure 31: Communal Amenity Space

Communal roof terraces have an important role in the provision of amenity within the residential elements of the Town Centre masterplan. The communal roof gardens and terraces shall complement the courtyard spaces, and provide residents with access to shared, high quality spaces that are easily accessed by residential cores, affording long views with appropriate wind mitigation measures, and shall create sunny spaces which are not overlooked or overshadowed (Figure 31).

The design of all roof spaces shall ensure that adequate integrated and free standing furniture will provide for passive recreational opportunities for residents. Spaces shall be flexible, to ensure a mix of uses are accommodated. Planting details and species shall be seasonal, appropriate to location, low maintenance and of suitable specification for the microclimate whilst also contributing to SuDs and biodiversity. Seating and planting shall be located to ensure the roof terraces can be divided up in a subtle and appropriate manner, with built-in, secure and safe storage and watering points for maintenance. Where necessary, wind screening shall be provided to ensure a suitable microclimate is achieved within roof level spaces. All boundaries shall be robust and shall not provide an opportunity to climb or access by garden users.

4.7 Micro Climate

Objective PD 12 of the Planning Scheme states: 'To ensure a sustainable built form with best practice sustainable design, construction methods and materials, which has regard to solar effect, wind tunneling prevention and microclimate. Adaptable residential building design, which is responsive to changing technical / economic and social conditions, is generally encouraged'.

Objective GI.5 of the Planning Scheme states: 'To require that the layout and orientation of residential areas ensures adequate levels of sunlight and good accessibility to all open space, in accordance with the requirements of Section 2.8'.

Amenity spaces, both as civic spaces and pedestrian and cycle streets within the public realm and communal residential areas, shall be designed to take advantage of positive microclimates and create attractive vibrant spaces. The design of amenity spaces shall seek to ensure adequate levels of sunlight penetration and as such avoid extensive areas of prolonged shade which often result in uninviting and unusable spaces. Similarly, the orientation of amenity spaces should avoid unnecessary exposure to the prevailing winds and the detailed design shall incorporate wind mitigation measures to provide shelter and minimize wind tunneling. Wind assessments shall accompany planning applications and clearly demonstrate how the design incorporates any proposed mitigation measures. For all amenity spaces, the usable amenity shall be located in areas that have good sunlight access and wind shelter, whilst avoiding circulation routes and utilities, all of which shall be considered at concept design stage.

In terms of communal amenity space associated with residential development, proposals shall seek to apply the guidance as set out in 'Sustainable Urban Housing: Design Standards for New Apartments Guidelines for Planning Authorities', DECLG, 2015 and 'Guidelines for Planning Authorities on Sustainable Residential Development in Urban Areas (Cities, Towns & Villages)', DEHLG, 2009 and 'Quality Housing for Sustainable Communities', DEHLG 2007. Design proposals shall ensure that adequate levels of light penetrate the communal amenity spaces throughout the year. However, having regard to the objective to create strong streetscape elements and achieve a successful urban form in the Town Centre, roof gardens may also be provided and can offer a satisfactory alternative and / or complementary space to the central courtyards, subject to climatic and safety factors being fully considered. Nonetheless, the central courtyard space is particularly appropriate for children's play and as such, the design of courtyard spaces should also ensure that noise reverberation is mitigated.

Chapter 5: Access And Movement Strategy

5 Access & Movement Strategy

Following specifically from the principles contained in Section 6.2 (i) and (j) of the Cherrywood Planning Scheme and its requirement to identify: vehicular and pedestrian routes within the Town Centre, and associated finished levels and access points including car park access points, section drawings; identification of the car parking spaces to be provided, the location access to and usage of car parks, including shared use car parks, and any on-street parking; together with those principles required by Section 6.2 (d), (e), (g) and (h) which refer to: finished floor levels of roads and footways and horizontal and vertical connections between Grand Parade; the location, design and treatment of squares and the public realm generally; the treatment of Grand Parade; and the provision of a separate pedestrian/cycle bridge across the WLR (further detailed within Sections 3.0 and 4.0), the Access and Movement Strategy for the Town Centre has been formulated. This Strategy is predicated on Objectives DA10, DA11, DA13, DA14 and DA15 of the Planning Scheme, the key elements of which are set out within this section:

DA10: 'The design and layout of the Town Centre shall address the following issues:

- In relation to Ground Levels, the change in the levels between the surrounding road network and the Town Centre Link shall be designed in a manner that is attractive to the pedestrian.
- In relation to the Grand Parade it is proposed as a low speed (30km/h) street facilitating movement across the Wyattville Link Road for pedestrians, cyclists, Luas and motorised traffic. It will mitigate the severance caused by the Wyattville Link Road and encourage interaction between both sides of the Town Centre. In traffic terms it will provide vehicular access to development blocks TC2 and TC4 and reduce at-grade crossing movements of the Wyattville Link Road.
- In relation to the relationship with the Wyattville Link Road, lands that encroach/about or in any way have a relationship with the Wyattville Link Road, shall show the relationship between all the building edges and the road. The design of all elements shall be such that pedestrian movement is positively discouraged across the Wyattville Link Road at-grade.

DA11: 'The design of the Town Centre link shall incorporate features to protect pedestrian and cycle movement. These features will provide a safe environment for all users of the Town Centre Link and the Wyattville Link Road'.

DA13: 'To design civic spaces to assist movement through the Town Centre, in particular marking where pedestrian routes converge or cross'.

DA14: 'That Tully Park is developed in accordance with the objectives set down in Chapter 5 Green Infrastructure and Chapter 3 Cultural & Built Heritage'

DA15: 'To provide a Transport Interchange (T.I) in accordance with Chapter 4 and Map 6.2.'

Note: In relation to DA13, pedestrian permeability is denoted through the Town Centre Quadrants on Map 6.2 of the Planning Scheme with a pedestrian bridge provided over the Wyattville Link Road in addition to the Grand Parade. DA14 and 15 relate to Tully Park and the Transport Interchange (T.I) located outside of the UFDF but are addressed insofar as appropriate for the overall design for TC1 and TC2.



Map 8: Pedestrian Movement Strategy

The Cherrywood Town Centre is the most important area of Cherrywood in terms of access to transport networks. Therefore, the design of the street network and public spaces in the Town Centre has a strong focus on sustainable transport modes, with priority given to the design of pedestrian and cycling facilities, thereby contributing to the achievement of the target modal splits set-out in the Cherrywood Planning Scheme.

5.1 Pedestrian Movement Strategy

Cherrywood Town Centre will accommodate significant pedestrian volumes associated with the proposed mix of uses. The 2 no. Luas stops located within the Town Centre will also generate pedestrian trips from outside the area.

The proposed pedestrian network is required to consider a number of constraints, including:

- The barrier to pedestrian movements posed by the Wyattville Link Road;
- Reconciling existing and new ground levels in particular in an east-west direction through TC2 and TC4; and

- The potential conflicts with other modes and infrastructure such as cycling, public transport and vehicular traffic, which also need to be provided in and around the vicinity of the Town Centre.

Accordingly, the Town Centre Pedestrian Movement Strategy has been prepared to comprise the following main elements (Map 8 Pedestrian Movement Strategy):

- A network of pedestrian links within each of the 4 no. Town Centre quadrants, connecting all relevant external and internal desire lines;
- The routing of legible links which by design clearly direct pedestrians and cyclists appropriately;
- A pedestrian-friendly boulevard on Grand Parade, where the need for pedestrians to cross the Luas tracks and the carriageway is carefully considered;
- Grade-separated pedestrian crossings of Wyattville Link Road provided through 2 no. high quality pedestrian facilities associated with Grand Parade and a dedicated footbridge between TC1 and TC3;
- Adequate pedestrian facilities incorporated in the design of junction A (Wyattville Link Road/Tully Vale Road /Cherrywood Avenue);
- Adequate pedestrian facilities incorporated in the design of junction A2 (Bishop St/Grand Parade).
- Any proposed pedestrian route to facilitate connectivity from TC2 across Tully Vale Road to the neighbouring existing residential area of Gleann na Rí (as illustrated on Map 8) will have due regard to safety and permeability at detail design stage.

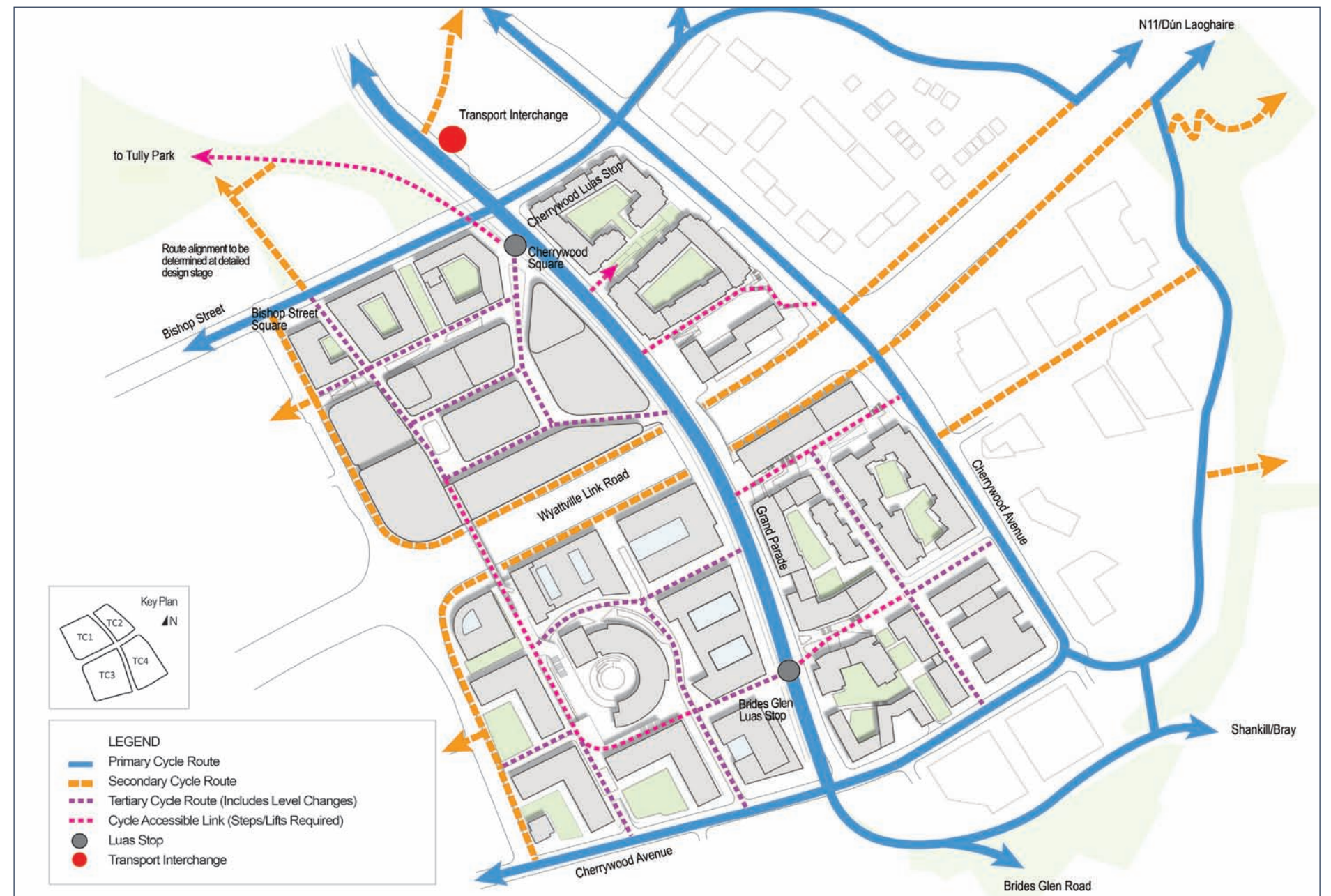
5.2 Cycle Movement Strategy

5.2.1 Cycle Connections

The road links within and surrounding the Town Centre will be provided with dedicated cycle facilities. This includes designated cycle lanes on Cherrywood Avenue, segregated cycle tracks on Cherrywood Avenue and Grand Parade, as well as combined bus and cycle lanes (1.5m and 3m) on Bishop Street and Tully Vale Road. Two-way segregated cycle tracks will be in place along either side of Wyattville Link Road along its Town Centre section (Map 9).

As with the pedestrian network, the Wyattville Link Road is a significant barrier to north-south movement for cyclists. North-south cycle movements will be mainly concentrated along the high-quality cycle tracks on Grand Parade, which provides a grade-separated connection between the northern Town Centre quadrants (TC1 and TC2) and the southern quadrants (TC3 and TC4). These facilities will also cater for the links between the north of the Cherrywood SDZ Planning Scheme area, mainly residential, and the south of the Wyattville Link Road, which will concentrate the majority of the area's employment.

In addition to Grand Parade, junction A will provide integrated cycle facilities and crossings integrated with a layout that responds to the key traffic role performed by this junction.



Map 9: Cycle Movement Strategy

5.2.2 Cycle Parking

Cycle facilities and parking shall be based on the current DLRCC Standards for Cycle Parking and associated Cycling Facilities for New Developments. This document distinguishes between long-term cycle parking, which is related to employment, retail and non-retail, and residential (cycle storage), and the short-term needs of visitors to all uses. Long-term cycle parking should mainly be accommodated within the covered secure areas such as basements and at Gateway entrances to the Town Centre, including at the Transport Interchange and Tully Park Link/Bishop Street Square, whereas the short-term parking should be located on surface ground levels close to building entrances and associated with the Luas stops, including the Transport Interchange, in convenient, well-lit places with passive surveillance whenever possible. Innovative solutions shall be used for cycle parking to ensure that public amenity spaces are not negatively impacted upon.

Within TC1, cycle parking provision shall be located at convenient and publicly accessible locations to access all uses. Particular consideration shall be given to encourage cycle access to community, civic and leisure uses, and as such appropriate levels and locations for cycle storage shall be provided. Cycle parking shall be located close to the Cherrywood Luas stop, Transport Interchange and Tully Park / Bishop Street Square Link.

Within TC2 and TC4, which will predominantly comprise of residential and employment uses, the majority of cycle parking provision shall be located within appropriately designated areas at basement level, close to lifts and stair cores, with visitor and short stay provision at ground floor level both on public streets close to entrances and within private areas associated with the residential blocks. Provision at surface level shall also cater for the short stay visitor to the non-retail uses and at Bride's Glen Luas stop.

Within TC3, for residents, locations for secure cycle parking will generally be provided in storage areas close to lift and stair cores with direct street access at Capital level 52 OD. For employees, locations for long-stay cycle parking areas with dedicated shower, changing and locker/drying facilities will be located primarily at basement level with dedicated access for cyclists. For visitors, locations for short-stay cycle parking will be located close to building entrances and in the public areas close to entrances to retail, non-retail and community uses, with spaces for further parking at Bride's Glen Luas stop. Dedicated proposals will be developed as part of planning application process.

Basement cycle parking shall also provide shower, changing facilities and locker/drying rooms, including employment areas.

5.3 Public Transport Movement Strategy

The Town Centre Public Transport Movement Strategy, including the strategy for both buses and Luas, is illustrated on Map 10. Cherrywood Town Centre includes 2 no. Luas stops: Bride's Glen to the south and Cherrywood to the north of the Town Centre. These Luas stops, along with the infrastructural requirements as set-out by Transport Infrastructure Ireland (TII), shall be integrated into the design of Grand Parade. The design of Grand Parade and adjacent buildings accommodates the potential future upgrade of the Luas to Metro, which will be reflected in longer platforms and a wider circulation corridor. These objectives have been considered in the overall urban form and block layout within the UDF with consideration given to street-widths, building set-backs, landscaping and street furniture.

High-quality pedestrian infrastructure and cycle parking for commuters is provided for within the design. Furthermore, service vehicle access to each of the stops is available off the proposed streets or by using parts of the adjacent hard landscaping, if necessary. 2 no. "kiss and ride" drop off points are allowed for on either side of Cherrywood Avenue close to Bride's Glen Luas stop. The exact location and design shall be determined at planning application stage. It is not intended to provide a set down facility adjacent to the Cherrywood Luas stop as to do so would impact negatively on either the quality of the cycle route along Grand Parade or the Quality Bus Corridor on Bishop Street. A small set down bay could be implemented on the east side of Grand Parade for "kiss and ride" at the Cherrywood Luas stop should the need arise.

The northern areas of the Town Centre are served by bus stops off Bishop Street and Tully Vale Road, which avail of bus lanes as well as the use of upstream and downstream bus gates (at the Bishop Street/Castle Street junction). These are likely to be the stops with highest frequencies in Cherrywood. The bus stops are to be provided in tandem with adequate pedestrian crossings, so as to ensure safe and convenient pedestrian access to and from.

The bus stops at Bishop Street form part of the Cherrywood Town Centre Transport Interchange, which will also include the Cherrywood Luas stop, a bus bay, a taxi rank and cycle parking located on Grand Parade immediately to the north of Bishop Street, all connected by high quality pedestrian links.



Map 10: Public Transport Movement Strategy

5.4 Vehicular Access and Movement Strategy

5.4.1 Road Hierarchy

Map 4.5 of the Cherrywood Planning Scheme identifies the road hierarchy for the main road network throughout Cherrywood. Typical cross-sections are prescribed for each type of road on Figure 4.4 of the Cherrywood Planning Scheme. Map 11 of the UFDF reproduces this road hierarchy in the Town Centre area and Figures 16(a)–(e) contained within Section 4.0 'The Public Realm Strategy', provides further detailed sections of Grand Parade.

Within the Town Centre, the road network is dominated by the Wyattville Link Road, which bisects the Town Centre. This road, a wide dual-carriageway, is the only 'Level 1' link provided within Cherrywood. This route, within the Town Centre has one full movement junction, at location 'A', to mitigate the potential impact of queuing traffic extending back onto the M50. The Town Centre must cross this road at the elevated level of the Luas.

Three of the four edges of the overall Town Centre are defined by 'Level 2' links defined as 50km/h roads: Tully Vale Road to the north of TC1/2 (north of Wyattville Link Road); and Cherrywood Avenue which loops around TC3/4 between the two Wyattville Link Road junctions.

The north western edge of TC1 and TC2 is defined by Bishop Street, which is a 'Level 3' 30km/h street. Grand Parade, which is intended to be the mobility spine of the Town Centre, is also a 'Level 3' 30km/h link, which runs parallel to and at the same grade as the Luas.

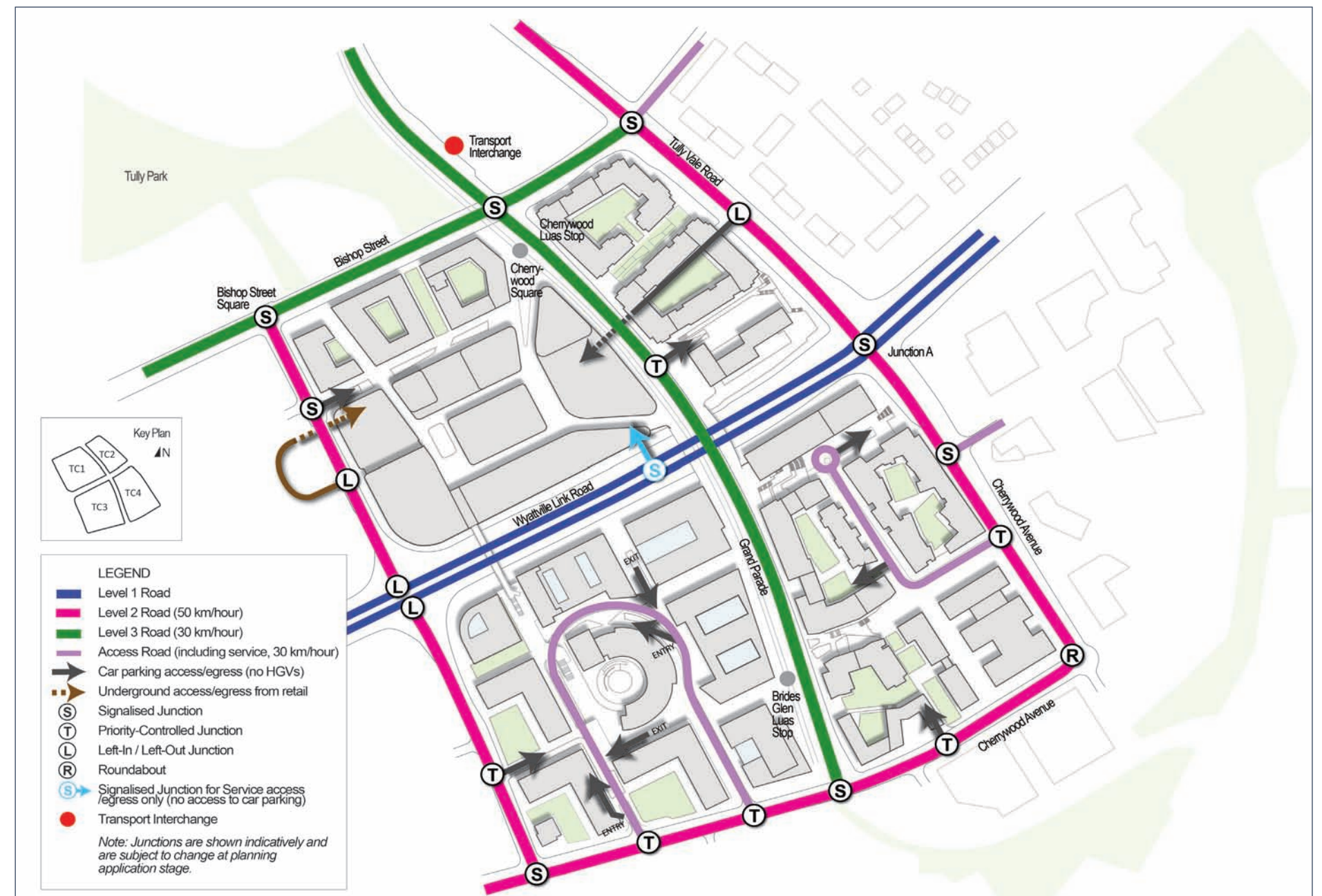
In addition, the UFDF now makes provision for vehicular internal streets, (Access Road on Map 11) particularly on TC3 and TC4 that respond to their specific design and requirements. These internal streets will be designed to accommodate the requirements of each quadrant, in the context of the guidance provided by the Design Manual for Urban Roads and Streets (DMURS) on 'shared space' principles.

As a result of the strategy to develop from the new ground levels and place cars and servicing beneath the podium, it is proposed that surface vehicular access to TC1 and TC2 will be mainly associated with emergency and occasional service and maintenance needs, while that to TC3 will also provide taxi set-down/pick-up areas close to building entrances. TC4 provides for an internal street to access the below podium car parking to the north of the plot.

Cherrywood Avenue

The UFDF provides for Cherrywood Avenue to be a two lane road with a central reserve that accommodates right turn lanes when required, with adjacent cycle tracks between the carriageway and a wide planted footpath with an overall width of 21.1m as shown in Figure 4.4 of the Cherrywood Planning Scheme.

The overall street width allows provision for in-line bus stops as shown in the National Cycling Manual Section 5.1.



Map 11: Vehicular Access and Movement Strategy

With regard to the part of Cherrywood Avenue that adjoins TC3, HIE2 and block G2, the UFDF proposes that pedestrian paving and cycle lanes are continuous across access roads consistent with best practice as set out in DMURS. It is intended that high quality material surfaces and finishes including kerbing, paving, planting, street furniture and lighting consistent with that for the rest of the Town Centre will be incorporated as part of the street design to ensure that an urban 'street' feel is obtained. Some limited car drop-off spaces for taxis and people with disabilities at building entrances are recommended and will be considered further with planning applications for building quadrants.

Bishop Street

This link accommodates a Quality Bus Corridor and as such, it provides a combined bus/cycle lane [1.5m and 3m] between the carriageway and a 3.0m wide footpath that is planted with street trees. Two pedestrian crossing points that link the Town Centre to Tully Park and the Transport Interchange are located at Cherrywood Square and Bishop Street Square respectively. The road surface at these locations shall reflect the importance of these pedestrian routes to provide a seamless visual and physical north-south link for the pedestrian.

Grand Parade

The section of Grand Parade through the Town Centre is intended to be an attractive public space, whilst providing important connectivity for walking and cycling across the Wyattville Link Road (See also Section 4.3.1 and Figure 24). The Planning Scheme details a cross-section of Grand Parade. However, subsequent analysis indicates that to accommodate future-proofing for Metro, there will be a requirement for a greater width of 7.1m for the Luas/Metro Corridor. Meanwhile, the provision for pedestrian/cycle crossing of the Luas track will be limited and well- defined. In this context, the cross-section shall include the following, from west to east:

- A footpath that will be a minimum of 3m wide;
- A Luas/Metro corridor of 7.1m in width; quadrants, connecting all relevant external and internal desire lines;
- A central landscape strip of a minimum 2.7m in width, with strategically located breaks to enable pedestrian movements across;
- A 5.5m carriageway bounded by 2.25m segregated cycle tracks/lanes on either side;
- A 3m-wide (minimum) footpath.

The layout may vary along the length of this section of Grand Parade in order to accommodate the requirements of the Luas stops and platforms. The cross-section described above will also differ at points where it integrates with Primary and Secondary squares along Grand Parade identified as part of the Public Realm Strategy (See also Section 4.0 and Figures 16a-g).

5.4.2 Car Parking Provision

The Planning Scheme set outs car parking standards for residential, high intensity employment (HIE) and retail uses (see Section 4.2.10, Tables 4.4, 4.5 and 4.6 refer).

Standards for residential developments are minimums. The minimum residential car parking standards are set out in Table 4.4. of the Planning Scheme. The minimum standard applicable to the Town Centre is 1 space per unit.

Standards for HIE is expressed as a maximum. HIE car parking provision will be permitted in accordance with Section 4.2.10 and as set out in Table 4.5.

Standards for retail uses are expressed as maximum and are set out in Table 4.6.

Non-retail uses should be primarily part of a linked journey or multi-purpose trip and therefore, the appropriate car parking standard will generally be 1 space per 50 sqm in such cases. However, it is acknowledged that some of the non-retail uses as end destination trips in their own right may have a requirement for additional car parking spaces, for example, a cinema, museum or places of worship. In such instances, the car parking requirements will be considered by the Planning Authority on a case by case basis and will be informed by the current Dún Laoghaire-Rathdown County Development Plan (See Section 8.2.4.5 Car Parking Standards of the 2015-2022 County Development Plan) and the Vision for Cherrywood as set out in the Cherrywood Planning Scheme, subject to the applicant sufficiently demonstrating the need for additional car parking. Any additional car parking spaces that may be considered appropriate for such uses by the Planning Authority may be segregated from all other car parking spaces and may have restriction imposed on their use.

Given the mixed-use and multi-purpose nature of the Town Centre, there is also the potential for shared or dual use parking provision where it can be demonstrated that peak demands do not coincide, especially where day and night uses are involved, e.g. day-time office use and evening entertainment or leisure activities. Applicants will be required to demonstrate that consideration has been given to the potential for shared parking. In the interest of clarity, car parking spaces provided for residential use shall not be shared with any other use.

The Planning Scheme gives consideration to the inclusion of a local park and ride facility (circa 200 spaces) subject to consultation with the NTA and TII and the availability of below podium space that can accommodate this use. The most likely location for this would be close to Bride's Glen stop.

Dedicated motor cycle parking spaces shall be provided at a minimum of four or more spaces per 100 car parking spaces.

Surface car parking will be limited throughout the Town Centre, particularly in TC1 and TC2 where parking will be accommodated below podium level. Some surface parking may be provided within TC3 and TC4 and along Cherrywood Avenue.

Car parking provision based on the likely quanta of development, in line with the aforementioned standards, is illustrated in Table 3 overleaf.

Table 3: Car Parking Provision		
Use	Planning Scheme Range - Tables 6.2.1 and 6.2.2 (sqm)	No. of car parking spaces required
TC1		
Retail Convenience /Food	2,357 – 4,740	Max 118 – 237
Retail Comparison	19,043 – 20,860	Max 381 – 417
Retail Services	5,350 – 6,400	Max 107 – 128
		Retail Sub-total: Max 606 – 782
HIE	8,000 – 10,000	Max 133 – 167
Residential	24,000 – 32,400 (255 – 346 units)	Min 255 – 346
Non Retail	17,000 – 21,000	See Note 4 Below
Community	1,100 – 1,500	Max 22 – 30
		TOTAL RANGE TC1 (excluding non-retail car parking⁽⁴⁾): Min 255 – Max 1,325 (see Note 6 below)
TC2		
Retail Convenience /Food	189 – 415	Max 9 – 21
Retail Comparison	1,531 – 1,825	Max 31 – 37
Retail Services	430 – 560	Max 9 – 11
		Retail Sub-total Max 49 – 69
HIE	0	Max 0
Residential	28,200 – 36,000 (300 – 384 units)	Min 300 – 384
Non Retail	7,000 – 9,000	See Note 4 Below
Community	0	Max 0
		TOTAL RANGE TC2 (excluding non-retail car parking⁽⁴⁾): Min 300 – Max 453 (see Note 6 below)
TC3		
Retail Convenience /Food	264 – 459	Max 13 – 23
Retail Comparison	2,136 – 2,021	Max 43 – 40
Retail Services	600 – 620	Max 12 – 12
		Retail Sub-total Max 68 – 75
HIE	54,800 – 74,300	Max 913 – 1,238
Residential	27,000 – 33,600 (287 – 358 units)	Min 287 – 358
Non Retail	8,000 – 10,000	See Note 4 Below
Community	1,100 – 1,500	Max 22 – 30
		TOTAL RANGE TC3 (excluding non-retail car parking⁽⁴⁾): Min 287 – Max 1,701 (see Note 6 below)
TC4		
Retail Convenience /Food	220 – 446	Max 11 – 22
Retail Comparison	1,775 – 1,961	Max 36 – 39
Retail Services	499 – 602	Max 10 – 12
		Retail Sub-Total: Max 57 – 73
HIE	12,753 – 17,453	Max 213 – 291
Residential	40,800 – 48,000 (434 – 512 units)	Min 434 – 512
Non Retail	15,500 – 20,000	See Note 4 Below
Community	0	0
		TOTAL RANGE TC4 (excluding non-retail car parking⁽⁴⁾): Min 434 – Max 876 (see Note 6 below)
	OVERALL TOTAL RANGE TC1, TC2, TC3 and TC4 (excluding non-retail car parking⁽⁴⁾)	Min 1,276 – Max 4,355

Notes:

¹ The quanta for retail convenience/food, retail comparison and retail services are calculated pro rata from “Table 6.2.1: Development Type and Quantum for Development Area 2 Cherrywood”.

² The above table does not include the allowance of 10% transfer of uses between the 4 areas in the Town Centre which is subject to agreement between the Town Centre Landowners. This may result in a corresponding transfer of car parking spaces.

³ The minimum residential parking provision takes into account the potential to achieve a greater number of units based on the Planning Scheme Amendments 1 - 4 (27 April 2017).

⁴ The non-retail uses should be primarily part of a linked journey or multi-purpose trip and therefore, the appropriate car parking standard will generally be 1 space per 50 sqm in such cases. However, it is acknowledged that some of the non-retail uses as end destination trips in their own right may have a requirement for additional car parking spaces, for example, a cinema, museum or places of worship. In such instances, the car parking requirements will be considered by the Planning Authority on a case by case basis and will be informed by the current Dún Laoghaire-Rathdown County Development Plan 2016 - 2022 (See Section 8.2.4.5 Car Parking Standards) and the Vision for Cherrywood as set out in the Cherrywood Planning Scheme. Car Parking spaces provided for residential use shall not be shared with any other use. Any additional car parking spaces that may be considered appropriate for such uses by the Planning Authority may be segregated from all other car parking spaces and may have restrictions imposed on their use.

⁵ The car parking ranges exclude car parking associated with non-retail uses.

⁶ The range for residential car parking is calculated based on the minimum standard of 1 space/unit. Therefore the total maximum value will be increased accordingly if car parking for residential use is provided above this minimum standard of 1 space/unit.

5.4.3 Car Parking Access Points and Circulation

The car parking access and circulation for the entire Town Centre at all levels is illustrated diagrammatically in Figures 32 (a) – (e).

TC1/ TC2

The basement underneath TC1 and TC2 shall be connected to provide increased flexibility for car park access. Access/egress to the TC1/TC2 basement car parking area shall be provided from Tully Vale Road, Grand Parade and Cherrywood Avenue (at 2 locations between Wyattville Link Road point J and Bishop Street). The Tully Vale Road access shall be a left-in/left-out arrangement to utilise the existing ground levels more effectively. It also has the benefit of reducing traffic in the Town Centre on Grand Parade and Bishop Street, thereby enhancing the pedestrian, cycle and public transport priority in these areas. The access and associated internal arrangements shall be designed to accommodate queuing traffic internally through

the provision of sufficient gate and stacking capacity within the car park itself, thereby preventing traffic queues extending onto the external road network. Variable message signs will be provided on approach routes to indicate real-time information regarding the availability of car parking spaces.

The majority of the parking spaces shall be accessed off Cherrywood Avenue (Access 1a & 1b) and Tully Vale Road (Access 1d). More limited access will be provided off Grand Parade (Access 1c) and this will be mainly for residential and hotel uses.

TC3

TC3 has 2 no. access/egress points (Access 3b & 3c) from Cherrywood Avenue South to Level +52m OD for taxi set-down/pick-up and access to the main parking and service areas, which include a dedicated residential parking area for Buildings TC3-7 and TC3- 8 at level 52m OD, and ramped access and egress to the two lower levels of parking and service areas.

A third vehicular access/egress (Access 3a) shall be provided off the road connecting Cherrywood Avenue with the Wyattville Link Road left-in/left-out (Cherrywood Avenue West). This shall provide limited access for taxi set-down/pick up and deliveries to buildings TC3-7, TC3- 8 and TC-9 which are primarily residential at street entrance level (+56.00m OD). It is envisaged that within TC3 most areas will be designed in compliance with the ‘shared spaces’ in compliance with DMURS.

TC4

TC4 will avail of 2 no. vehicular accesses off Cherrywood Avenue: one at the east of the site, leading to an internal street from where on-street and basement car parking can be reached (Access 4b & 4c); and one from the south, directly connecting with basement car parking (Access 4a).

5.4.4 Servicing and HGV Access

The strategy for access and circulation for HGV’s and servicing the Town Centre is detailed below and illustrated on Map 11 Vehicular Access and Movement.

TC1/ TC2

In order to reduce the need for service vehicles and HGVs from the internal road/street network and from along Tully Vale Road, service and HGV access to TC1 and TC2 shall be provided directly from the Wyattville Link Road in the form of a fully signalised junction (Access 1e). This thereby protects the quality of service afforded to sustainable modes of transport on the Town Centre network. The hours of operation of this junction may be restricted and will be considered at design stage in agreement with the Planning Authority and subject to the demonstration of traffic impacts on adjacent junctions on the Wyattville Link Road.

The Vehicular Access and Movement Strategy provides for a number of internal service/waste collection areas on the periphery of the basement connected by a double-height corridor, which will serve all the uses proposed for these Town Centre quadrants (Figures 32c &d).

A limited amount of needs can be catered for by drivable sections of the hard landscaping on surface. For example, these include the potential use of the Cherrywood Plaza area to service the Cherrywood Luas stop, or the hotel drop-off for occasional needs of the hotel.

TC3

The Vehicular and Access Movement Strategy for TC3 considers the service requirements for a variety of different uses. For HIE and commercial uses, waste collection points and plant areas shall be are located at basement level (Level 48). For residential areas, waste collection is at basement level (Level 52) of each building. For retail and non-retail, waste and plant is allowed for from the building with direct vehicular access at Level 52. A waste management centre is located within TC3 to allow for all waste to be brought to one point for collection.

A dedicated vehicular delivery and service yard at Level 52 is proposed to provide for the operational requirements of the retail/non-retail elements of building TC3-9 (Figure 32b). This is off-street and covered to minimise any noise or adverse impact for residents, particularly in the early morning hours when most deliveries and services are scheduled.

For residential buildings, limited service vehicle access is possible to the entrance level of each residential block for emergencies, deliveries, furniture moving, etc. so that heavy goods can be wheeled to building entrances and carried by lift to each floor.

TC4

This quadrant will avail of an internal street that shall include a service area associated with the office building. The retail element of the proposed development shall be catered for by a loading bay on Cherrywood Avenue, at the southern boundary of the plot (Figure 32a).

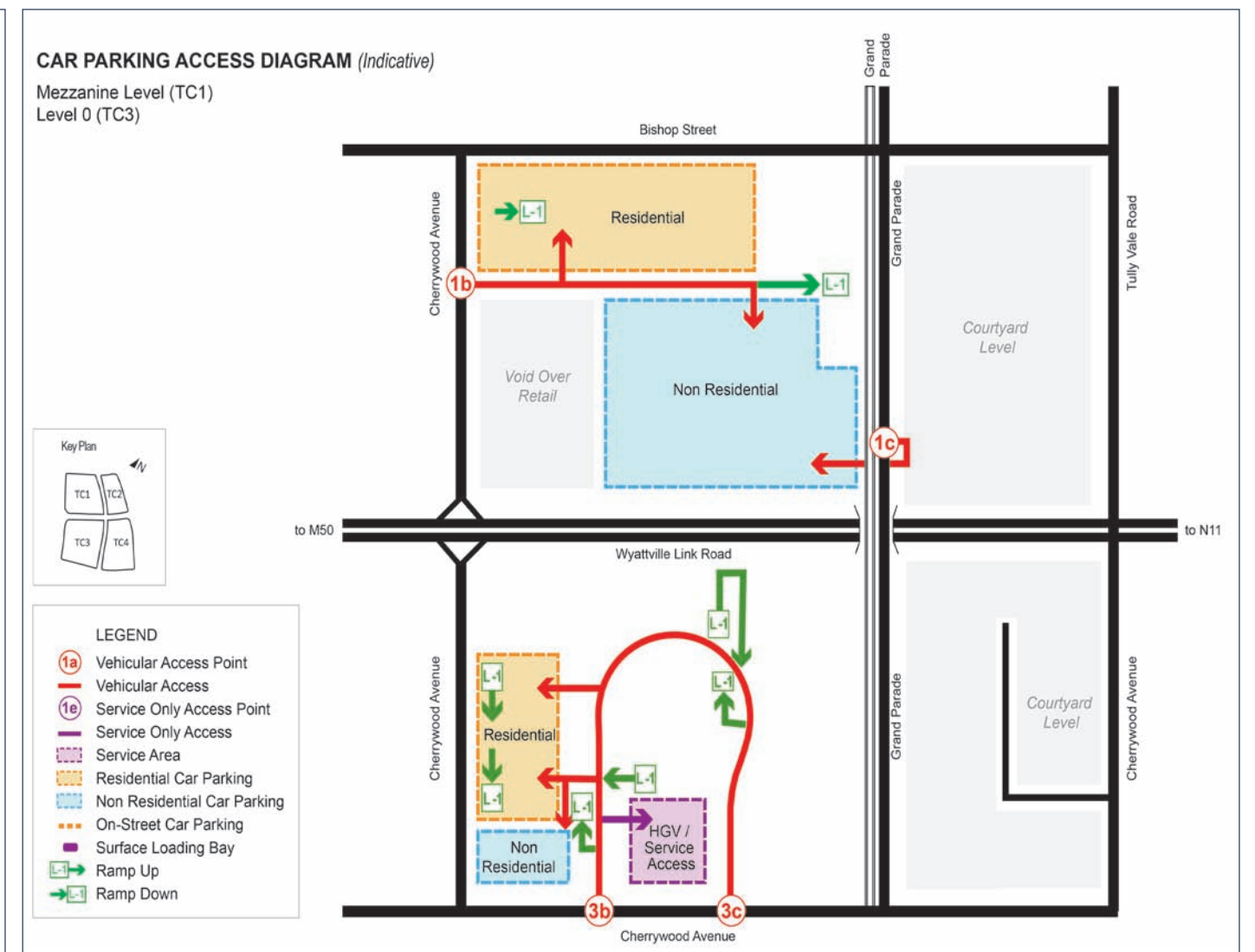
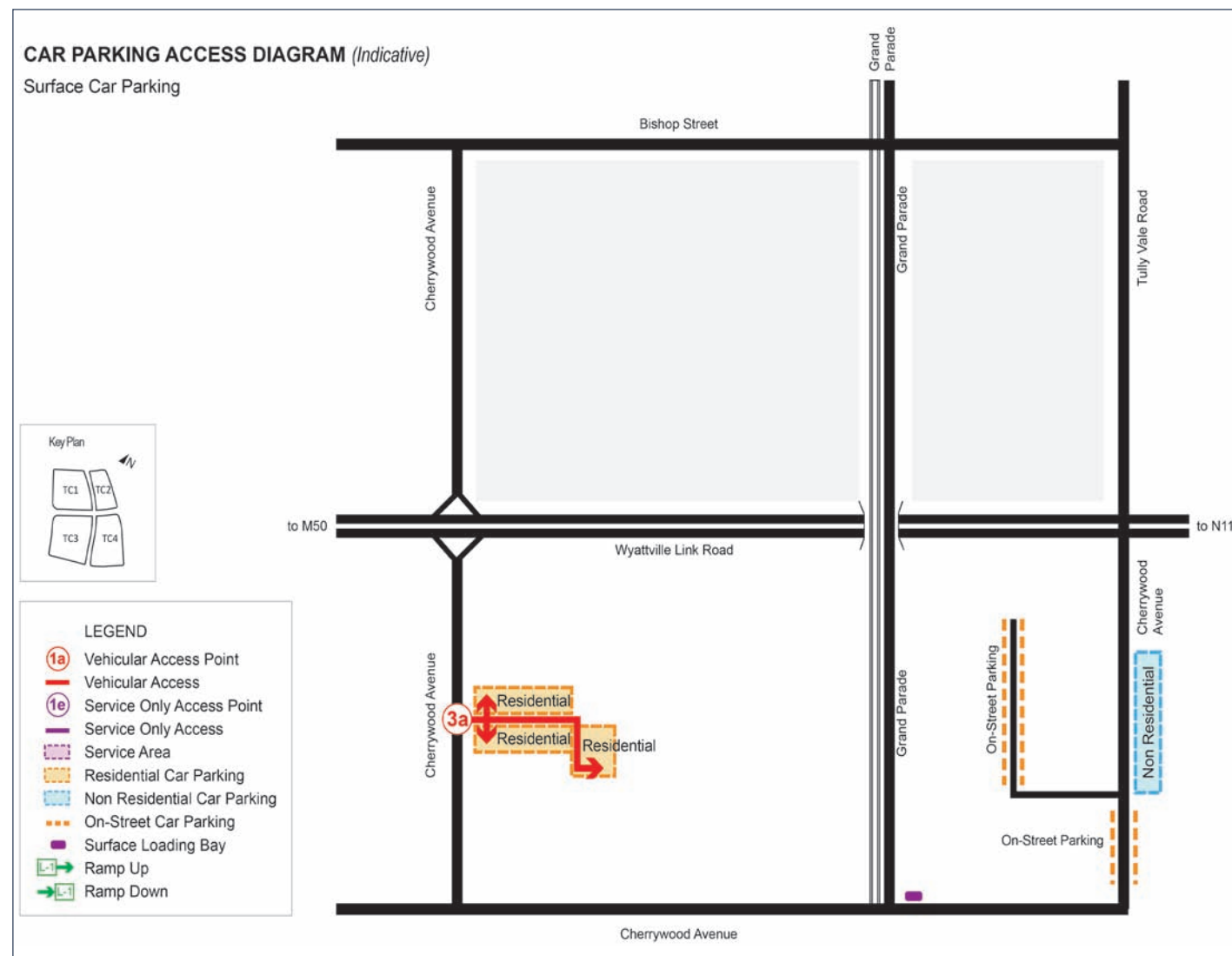


Figure 32a: Car parking Access and Circulation Surface Level

Figure 32b: Car parking Access and Egress Surface Level

CAR PARKING ACCESS DIAGRAM (Indicative)

Level -1

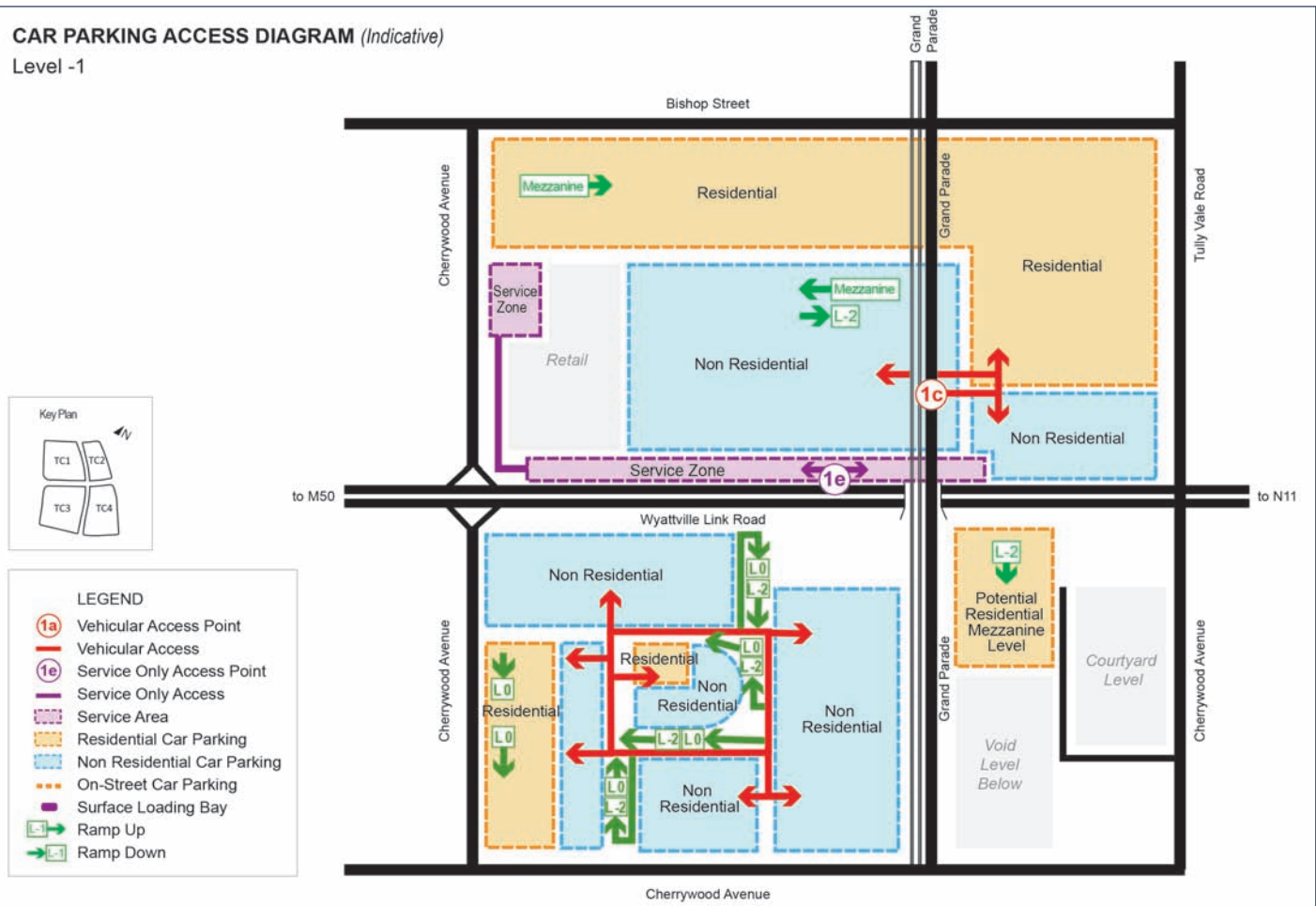


Figure 32c: Car parking Access and Circulation Level-1

CAR PARKING ACCESS DIAGRAM (Indicative)

Level -2

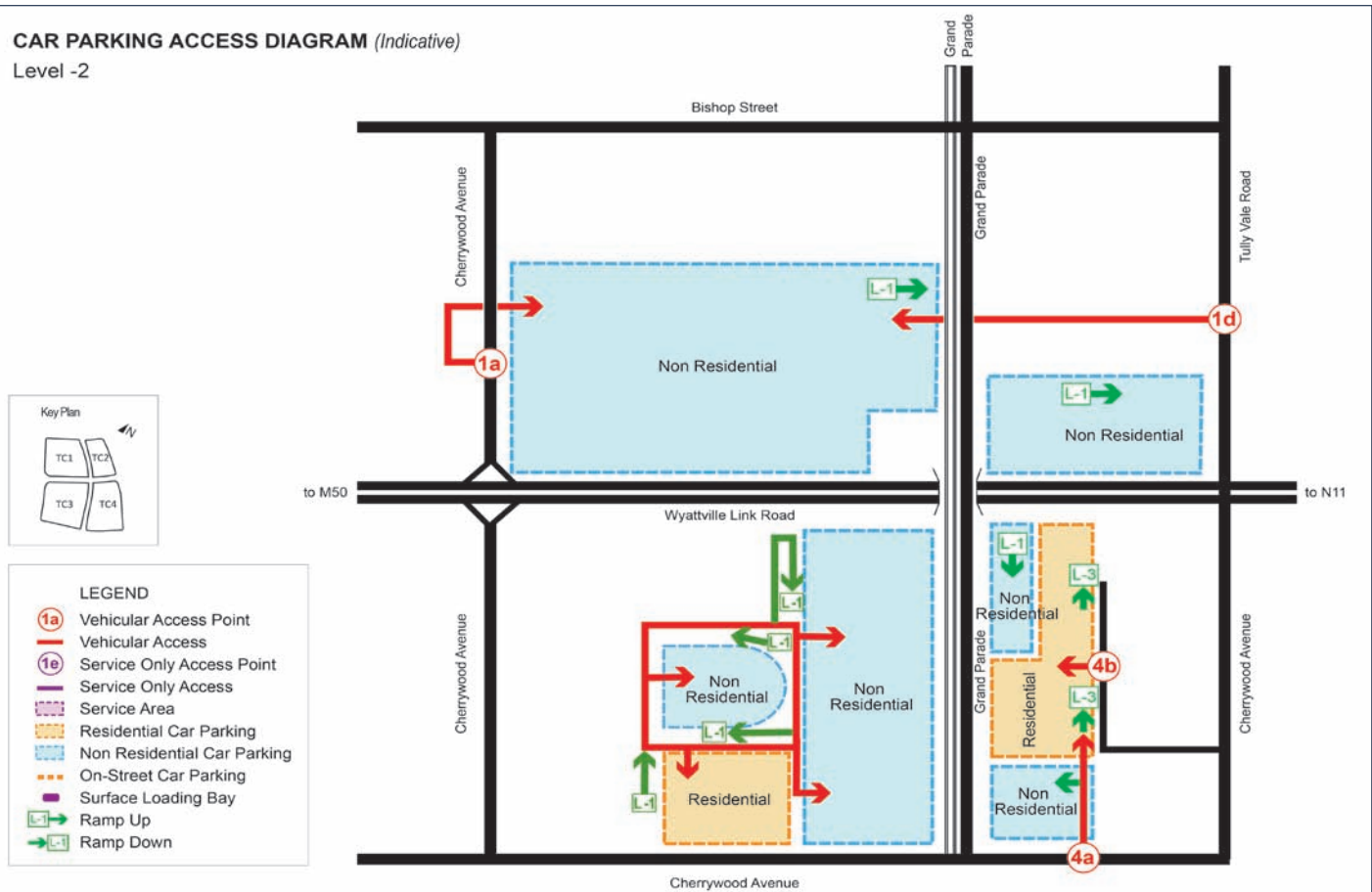


Figure 32d: Car parking Access and Circulation Level-2

Chapter 6: Delivery & Implementation

6 Delivery & Implementation

6.1 Delivery

Dún Laoghaire-Rathdown County Council will play a pivotal role in active land management in order to expedite the delivery of the Town Centre for Cherrywood, as the key centre of economic, social and retail activity within the Planning Scheme Area. Delivery of the Town Centre, as a mixed-use area as envisaged under the Planning Scheme and further articulated in the UFDF, is an important objective in terms of achieving the overall vision for Cherrywood as a sustainable place, with a rich urban diversity, a strong identity and a vibrant economic community anchored around the Town Centre.

In this regard, the Development Agency, will continue to actively liaise with the landowners and relevant statutory bodies to resolve any potential blockages in the delivery of the Town Centre.

However, ultimately, the delivery of the Town Centre will be largely dependent on the relevant landowners bringing forward their respective sites for development and demonstrating compliance with the UFDF and Planning Scheme as part of the development management process.

6.2 Implementation & Review

To facilitate implementation of the UFDF, as a critical component of the Planning Scheme, it is proposed that there will be on-going monitoring and evaluation of the UFDF as a comprehensive framework to assessment. In this regard, the Development Agency will liaise with all relevant landowners, representatives of relevant agencies and Government Departments, and will report on such matters to the Cherrywood SDZ Steering Group.

In this context, should the UFDF require subsequent revision and refinement, this will be done so by the Development Agency in consultation with the relevant landowners and in accordance with the Cherrywood SDZ Planning Scheme. As such any future amendments to the UFDF, shall not materially alter the Planning Scheme. In the event of any disagreement between the parties on any future amendment, the matter shall be referred to An Bord Pleanála for determination.

